

CASTLEMAINE CENTRAL AREA URBAN DESIGN FRAMEWORK



Prepared for Mount Alexander Shire

by

Planit

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Urban Design Framework**

**Prepared for Mount Alexander Shire
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SUMMARY AND KEY RECOMMENDATIONS

S1 Introduction

This is a summary of the Urban Design Framework for the Castlemaine Central Area. This study has been prepared in response to the economic, physical and cultural changes that are now occurring in the Central Area. Decisions need to be made about how and where new development should occur whilst preserving the urban form and character of the Central Area. Today the Central Area is a busier centre - there are more cars, shoppers and a more active street life. The Central Area offers more retail and service variety than in previous times. The activity focus in the Central Area has shifted with negative impact being felt in what were previously busy precincts.

A policy of containment within the historic centre has, with few exceptions, been successful. Little change has occurred at the edges of the Central Area. These edges are now feeling some pressure, as development opportunities become limited within the Central Area.

Tourist visits to Castlemaine and the Central Area are increasing creating demand for service and speciality retailing. Demand for retail and business services is changing in response to the changes that have been occurring in the Shire's population mix over the past two decades.

S2 Project Objectives

The project objectives as set out in the project brief are:

- Prepare a framework plan that provides a long-term vision for the Central Area.
- Identify land use options and strategies for key sites.
- Illustrate and describe design concepts for key and important sites.
- Set out an implementation strategy with priorities and timing including what can be achieved now, in the next five years and in the longer term.
- Recognise the fundamental significance of heritage to the integrity and character of the Central Area.
- Utilise existing relevant studies and build on this body of strategic work.
- Demonstrate an understanding of how the Central Area and its precincts function ie movement patterns by pedestrians and vehicles, availability of carparking, street life and activity, important viewlines etc. Opportunities to improve the function of the Central Area should be identified.
- Ensure that the urban design framework develops synergies with other programs and initiatives that have Council support.
- Identify any amendments to the planning scheme that would be required to implement the Strategy

S3 Structure of the Report

The report has four main sections.

Section 1 (Summary and Key Recommendations)

Section 2 (Strategic Context) provides an analysis of the salient urban design and planning issues that will influence the future function and performance of the Central Area.

Section 3 (Urban Design Framework) takes an overall look at the Central Area and advances urban design propositions relating to use and activity, vehicle and pedestrian access and car parking, opportunities for new retail and business development, and protection and enhancement of heritage buildings and streetscapes.

Section 4 (Precinct Framework) examines six precincts in more detail. Objectives and strategies have been formulated to implement the urban design framework and provide a strategic basis for decision making by Council.

S4 Background

Castlemaine Central Area is a regional shopping and business centre for a population catchment that generally equates with the Mount Alexander local government area. It services a population catchment of approximately 16,000 people.

The Central Area also performs a tourism role for tourists visiting the Shire, township, outlying towns and the goldfields region. Heritage buildings and streetscapes are of State and national importance. The entire Central Area is covered by a Heritage Overlay in the Mount Alexander Planning Scheme. Many individual buildings also have a heritage listing in the Planning

Scheme or on the Historic Buildings Register, National Estate or National Trust Register.

Castlemaine and district has experienced consistent population growth over the past two decades, with most new residents moving from Melbourne. Population growth is forecast to continue and may even accelerate with the proposed reductions in road and rail travel times to and from Melbourne.

An urban design framework is needed to identify new development sites in anticipation of market demand and to ensure that new and replacement buildings do not compromise the heritage character and integrity of the Central Area.

In summary the reasons behind the commissioning of this study are:

- A forward looking plan is needed to respond to anticipated demand for additional retail and commercial floor space.
- Council has received numerous enquires from businesses and retail developers about the availability of vacant land and retail and business floorspace within the Castlemaine Central Area.
- Council officers report that there is a lack of Central Area office locations for small to medium sized businesses that employ five or more employees.

Previous strategic work that has relevance to the Central Area Urban Design Framework are:

- Castlemaine Townscape Improvement Plan (Trevor Westmore 1989).
- Castlemaine Central Area Retail Study (USE Consultants 1995).
- Castlemaine Urban Design Strategy (Geoff Sanderson & Assoc 1998).
- Castlemaine Civic Precinct Action Plan (Projektion 1999).
- Mount Alexander Planning Scheme (1998).
- Castlemaine Town Centre: Parking Study (Andrew O'Brien & Assoc 1997)

S5 Central Area Concept Plan

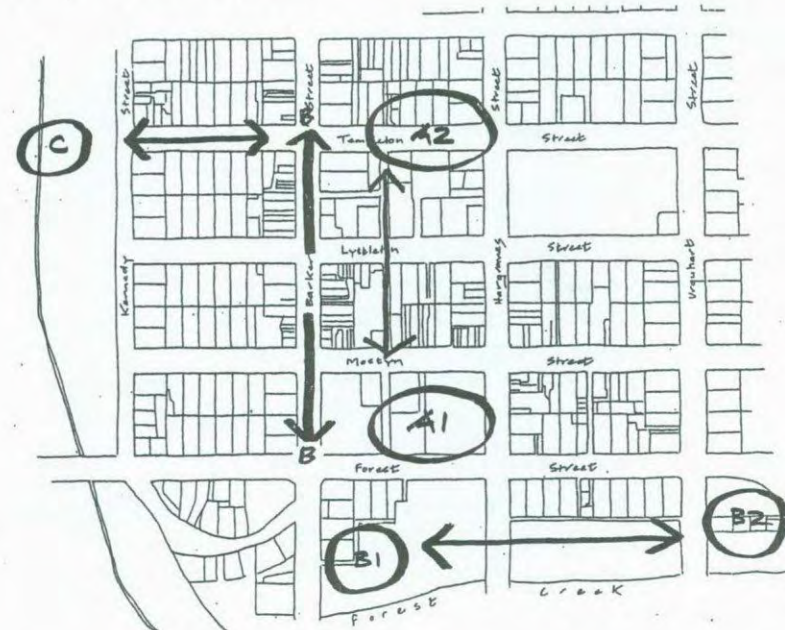
It is important that a long term vision for the Central Area is established. Planning for the Central Area must be integrated and coordinated according to an agreed strategy. Elements of this Strategy include:

- minimising walking distances for shoppers and visitors
- creating a vibrant Central Area
- protecting and enhancing heritage streetscape
- improving vehicle access and car parking
- providing opportunities for new developments that are well located

Integrated and coordinated planning can be achieved by applying basic planning principles that provide a structure for growth, balance and stability of the Central Area. A general layout is required as a first step. Retail anchors provide the form of the layout.

IGA Supermarket and the Tourist Information centre in Mostyn Street anchor the Central Area at one end. An anchor site is proposed at the northern end in Templeton Street. Shoppers and visitors visit shops on their travels between the major stores or anchors. Frederick Street and Hargraves Street are two major pedestrian links between the two anchors. Frederick Street can take on a greater role through redevelopment proposed within the two streetblocks it bisects.

The application of this basic planning principle is illustrated on Plan No. 1 Central Area Concept Plan.



- A1 Existing Anchor: IGA Supermarket
- A2 Proposed Anchor: Templeton Street (north & south side)
- ← B Barker Street Corridor
- B1 Proposed Anchor: Forest Creek/Street Precinct - west end
- B2 Proposed Anchor: Forest Creek/Street/Precinct - east end
- C Existing Anchor: Railway Station

Plan No. 1

Castlemaine Central Area Concept Layout

S6 Urban Design Framework

The urban design framework provides a longer term vision for the Central Area. Its purpose is to provide a coordinating strategy for future retail and commercial growth. The framework identifies opportunities in anticipation of market demand and recommends preferred land use and design outcomes at key sites. Access strategies for cars and pedestrians are central to the urban design framework.

The main elements of the framework are:

- Improving vehicle and pedestrian access and links
- Providing development opportunities for business
- Enhancing heritage and landmark sites
- Establishing a precinct framework for action plans
- Identifying key sites for new development/redevelopment
- Creating public space areas integrated with new developments
- Providing opportunities for urban art

These elements are covered in detail in Section 3 and 4 of this report. The Urban Design Framework is illustrated in Plan No2.

Key Recommendations

1. Concentrate future core retail within the area defined as the Central Core Precinct. See Plan No.14.
2. Encourage new retail and commercial development at three Central Core sites. These sites are identified on the Urban Design Framework Plan. See Plan No's 2, 15, 16 & 17. The application of Design and development Overlays Design and Development Overlays for each of the following sites is recommended.
 - Lyttleton St Frederick Lane Site (Site A2) (no net carpark loss)
 - Civic Precinct Site (Site A1)
 - Templeton Street



Fig 1: Barker Street Precinct



Fig 2: Ex Steam Flour building in Barker Street. Restoration and use of building recommended.

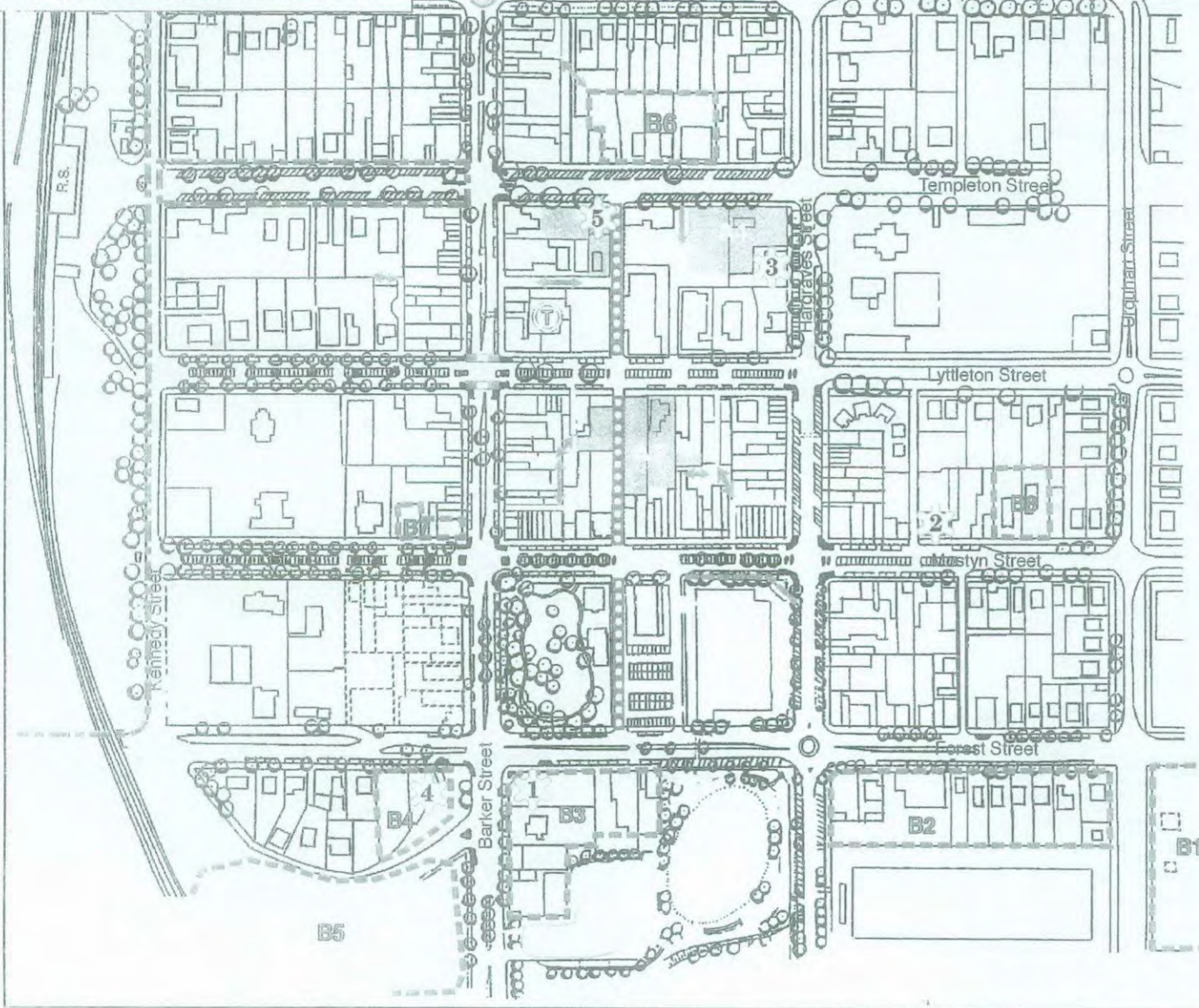
3. Develop Frederick Street as a major pedestrian link connecting to each of the new development sites in the Central Core Precinct.
4. Prepare a "Civic Precinct Local Policy " that articulates Council's vision for the Precinct including preferred uses for existing and any new buildings; and vacant sites within the Precinct. The Local Policy will be included in the Planning Scheme by amendment to the Scheme.
5. Encourage a major store and other retailing to locate at the northern end of the Central Core Precinct in Templeton Street at sites identified on the Urban Design Framework Plan.
6. Develop the Forest Street/ Forest Creek Precinct (See Plan No. 18) as a mixed use and self contained precinct. It is recommended that a Design and Development Plan Overlay be applied to the Precinct.

Main proposals include :

- Location of new retail, commercial or community buildings at the eastern and western end of the Precinct connected by pedestrian links associated with the Creek and existing recreation facilities.
 - The site east of Urquhart Street is recommended for retail/residential. A community or cultural facility is recommended for the western site between Barker Street and Western Reserve.
 - Identification of the southeast corner site of Forest and Wheeler Streets (Hunt and Lobb site) as a priority site for retail/commercial redevelopment. Preferred use of the site is for a retail use that is not dependent on a central location.
 - Longer term redevelopment of the properties with Forest Street frontage between Hunt and Lobb P/L site and Urquhart Street.
7. Restore vitality to Barker Street by a range of proposals relating to corner site redevelopment, traffic management and calming and identification of preferred uses for the precinct. Major specific proposals include :

- Introduction of traffic signalling at the intersection with Lyttleton Street. Investigate roundabout for Templeton Street in the longer term or to coincide with development of proposed development sites in Templeton Street.
 - Redevelopment of the two prominent corner sites; south east corner of Lyttleton Street and north west corner of Mostyn Street.
 - Encourage mixed use of shops on west side of Barker Street eg retail commercial, tourism retail, and speciality shops. Link uses with tourism retailing uses proposed for Railway Station Templeton Street West Precinct.
8. Encourage tourism retailing and other related tourism use along the Railway Station/Templeton Street West Precinct and improve vistas and view corridors to Railway Station and the Barker Street.
 9. Encourage progressive redevelopment of the Barker Street Southern Entrance Precinct to accord with a Precinct Concept Plan. It is recommended that a Design and Development Plan Overlay be applied to the Precinct with the Precinct Concept Plan as part of the Overlay Schedule.
 10. Plan for a longer term highway bypass of the Castlemaine Central Area to create a safer and more pedestrian friendly Barker Street Precinct.
 11. Preferred uses for the Southern Entrance Precinct are highway business type uses that do not depend on a central retail location.
 12. Seek funding for a Heritage Study of the Castlemaine Central Area. This Study would review the "Architectural and Heritage Study" (1979) and provide a historical context for new buildings, redevelopment proposals and capital works and urban design improvements proposed for the public realm.

**PLAN NO.2 : CASTLEMAINE
TOWN CENTRE URBAN
DESIGN FRAMEWORK PLAN**



- Key Development Areas & Sites in Central Core.** Apply Design & Development Overlay to identified sites. Includes provision for car parking & public space. Refer to Section 4 of Urban Design Framework (UDF) Report for detail on specific sites.
- B1 Potential Development/Redevelopment Sites & Areas.** Apply Design & Development Overlay to identified areas/sites. Includes provision for car parking & public space. Refer to Section 4 of UDF Report for detail.
- B1, B5: Retail/Residential mix
 - B2, B3: Retail/Office mix
 - B4, B6-8: Retail
- Implement Recommendations of Castlemaine Railway Precinct Master Plan.**
- Implement "Link to Town Centre" Recommendations.** (Castlemaine Railway Precinct Master Plan)
- Encourage Residential Mix** with retail & commercial in future development/redevelopment proposals.
- Proposed Pedestrian Access Spine**
Improve pedestrian and visual amenity.
- Minor Pedestrian Access**
Make connections through street blocks.
- Pedestrian Connection Required or Reinforced**
- Relocate Traffic Signals** to Lyttleton Street intersection.
- Proposed Roundabout**
Marking entry to town centre and to calm traffic through Barker St. precinct.
- Change in Use of Premises Recommended.** Refer to Section 4 of UDF Report.
- Relocate Telecommunications Building** - create Post Office Plaza
- Youth Activity Area**
- Remove Physical Barriers**
- make open to street
 - improve amenity of public space areas

2. STRATEGIC CONTEXT

2.1 Study Area

The study area is bounded by Forest Creek, Kennedy Street, Campbell Street and Urquhart and Hargraves Streets. The study area is shown on Plan No. 3.



Plan No. 3 Study Area

2.2 Population Growth

Mount Alexander Shire's population is projected to increase by 890 persons over the next 20 years (See Table 1). The number of households is projected to increase by 1,700 over the same period. The projected increase in number of households will be the result of declining household size.

These projections may be on the conservative side. Higher future population levels may be experienced in the Shire with the introduction of a faster rail service between Melbourne and Bendigo and the eventual duplication of the Calder Highway. Reductions in travel time will increase levels of commuting to Melbourne. The Shire may experience the type of growth rates that Macedon Shire towns have experienced over the past ten years.

**Table 1. Population & Households 1996-2021
Mount Alexander Shire and Castlemaine**

	2001	2011	2021
Shire			
Total Population	16,170	18,690	21,390
Households	7,030	9,120	10,970
Average Annual Population Change		2001-11 1.4 %	2011-2021 1.4%
Castlemaine			
Total Population	7,950	9,200	10,500
Households	3,310	4,000	4,770

Mount Alexander Urban Living Study, 2004

Population growth in Castlemaine and the Shire will generate demand for additional floorspace and increase shopper activity in the Central Area.

2.2 Tourism

Tourism is a growing industry in Castlemaine and Mount Alexander Shire. The Castlemaine Visitor Information Centre recorded a total of 95,340 visits for the year 2000. In 2003 126,000 visits were recorded.

A total of 306,000 day trips to the Shire were recorded in 2002. In 2002 there were 99,000 overnight visitors and 183,000 nights spent in the Shire. The value of overnight visitation was estimated to be worth \$14 million to the local economy in 2002. (Mount Alexander Shire Tourism Profile 1999-2002, Mount Alexander Shire Tourism Master Plan, 2004)

The number of tourists visiting the Central Area has increased noticeably, particularly on weekends and public holidays. The relocation of the tourist information centre to the Market building in Mostyn Street has succeeded in bringing tourists and visitors to the centre of the town. Tourists and visitors need to be able to direct themselves around the town to places of interest. Providing a variety of ways to get to places on foot will enhance the experience of discovering the attractive qualities of the Central Area. A range of initiatives can be pursued including:

- Improved signage
- Creating vistas
- Open and closing views with trees or buildings
- Theme street planting
- Pavement design and use surface materials and finishes.

2.3 Consolidation of the Centre

Accommodation of new retail and commercial floorspace by consolidation within the existing Central Area is the model supported in the Mount Alexander Planning Scheme.

The Commercial Development Local Policy (22.01) specifies that:

"the primacy of the Castlemaine Central Business District is to be protected and redevelopment of land within the existing retail area is to be encouraged".

The Local Policy implies that consolidation of the Central Area should generally occur before expansion at its edge. The Planning Scheme does not support the location of major stores and concentration of retail stores at suburban Castlemaine locations.

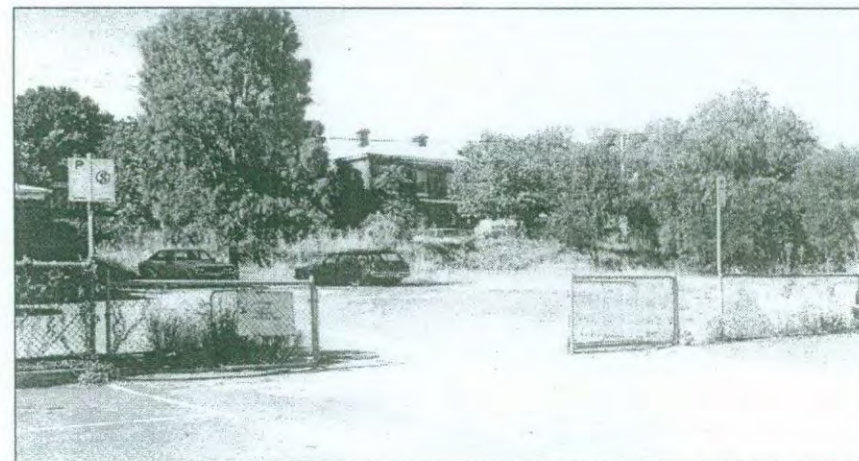


Fig 3: Rear of Town Hall. Potential development site with frontage to Templeton Street

Statement of Heritage Significance

A statement of heritage and cultural significance has not been prepared for the Central Castlemaine Conservation Area (HO667). The Castlemaine Townscape Improvement Plan (T. Westmore 1989) describes the cultural significance of Castlemaine (ie. former City of Castlemaine) as follows:

“ The City’s historic and architectural significance is generally accepted as of State wide (probably National) importance exemplified by the great attention by publications on historic places and the numerous listings of buildings and sites by the Historic Buildings Council, the National Estate and the National Trust.”

2.4 Future Demand for Central Area Floorspace

The Castlemaine Central Area Retail Study in (U.S.E Consultants, 1995) estimated that the retail expenditure pool for the Castlemaine region was \$75 million in 1995. This was projected to increase to \$105 million by 2011.

The Central Area Retail Study concluded that there is definite scope for additional small speciality stores and a medium sized store. eg 1,000 sq.m of market positioned mixed goods. The Retail Study also identified the need for a new supermarket, a need that has been met with the establishment of the IGA supermarket.

The Urban Design Framework has a timeframe to the 2020. Allowance for additional floor space is required beyond the forecast period of the Retail Study. Allowance also needs to be made for higher population growth rates that may occur as a result of reduced road and rail travel times to and from Melbourne.

It is important that an agreed framework for future growth in the Central Area is adopted so that decisions in the shorter term do not compromise the longer term vision and plan.

2.5 Urban and Built Form

Heritage streetscapes are integral to the Central Area's form and character. Conservation and enhancement of the existing urban form and character must be regarded as essential and the most cost-effective means of vitalisation. Where new developments are contemplated they must respect the form and character. Whilst new developments must be sympathetic to existing heritage streetscapes, there should also be scope for contemporary architectural expression.

Castlemaine was originally surveyed by Robert Hoddle. The layout of the Central Area follows the traditional grid pattern. Large street blocks (100m x 200m) and wide reservations are two outstanding features of the Central Area's urban form.

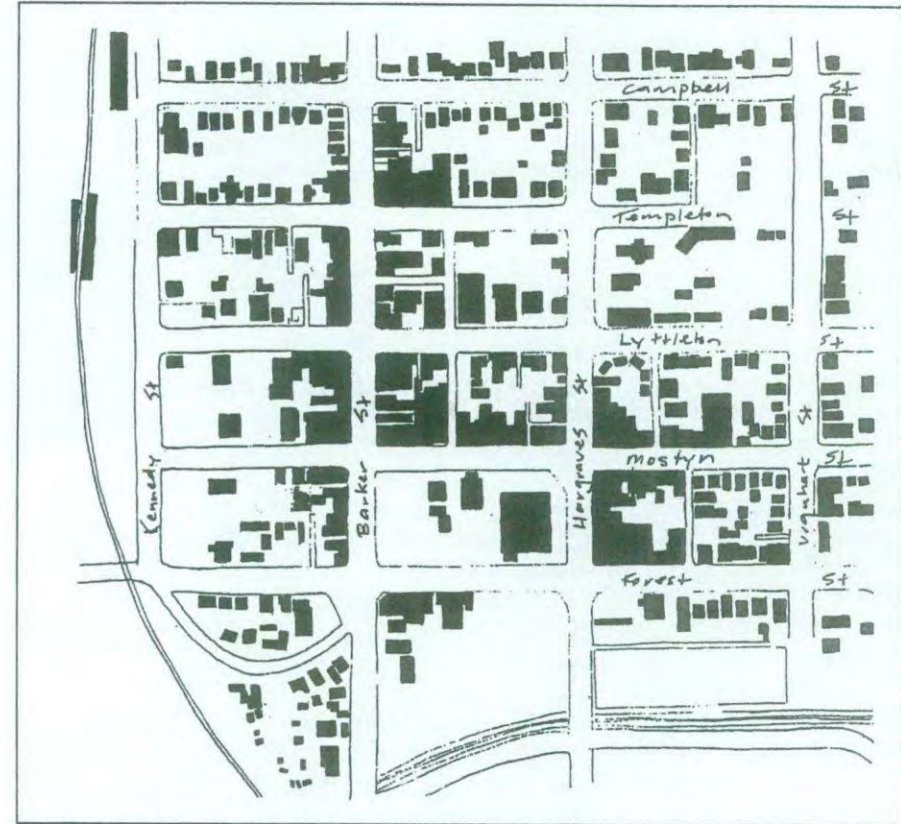
The laneways that permeate these street blocks are a feature of the Central Area's urban form. Largely unused, these laneways can provide a variety of ways of moving through the centre and getting to places.

Frederick Lane bisects two of the major street blocks. It has the potential to be a major pedestrian axis with connections to places of activity and interest. Dispersal of pedestrian flows through the street blocks can help to create more balanced business opportunities within the Central Area.

The arrangement of heritage buildings and public open space within the Market Square Precinct and Civic Precinct is a characteristic of the Central Area's urban form. Frederick Lane connects these two street blocks.

The Midland Highway (Barker Street) and the Pyrenees Highway (Forest Street) form the eastern and southern boundaries or edges. The highways impose a fundamental constraint but also provide opportunities for physical improvements in the Central Area.

The heritage and built form character of the Central area is illustrated in Plan No. 4. The plan below illustrates the relationship between freehold land and public space. The Plan highlights the network of right of ways and laneways and their connections to major streets.



Plan No. 4 Figure Ground Plan, Castlemaine Central Area

2.6 Constraints and Opportunities

Constraints and opportunities are illustrated on Plan No. 5.

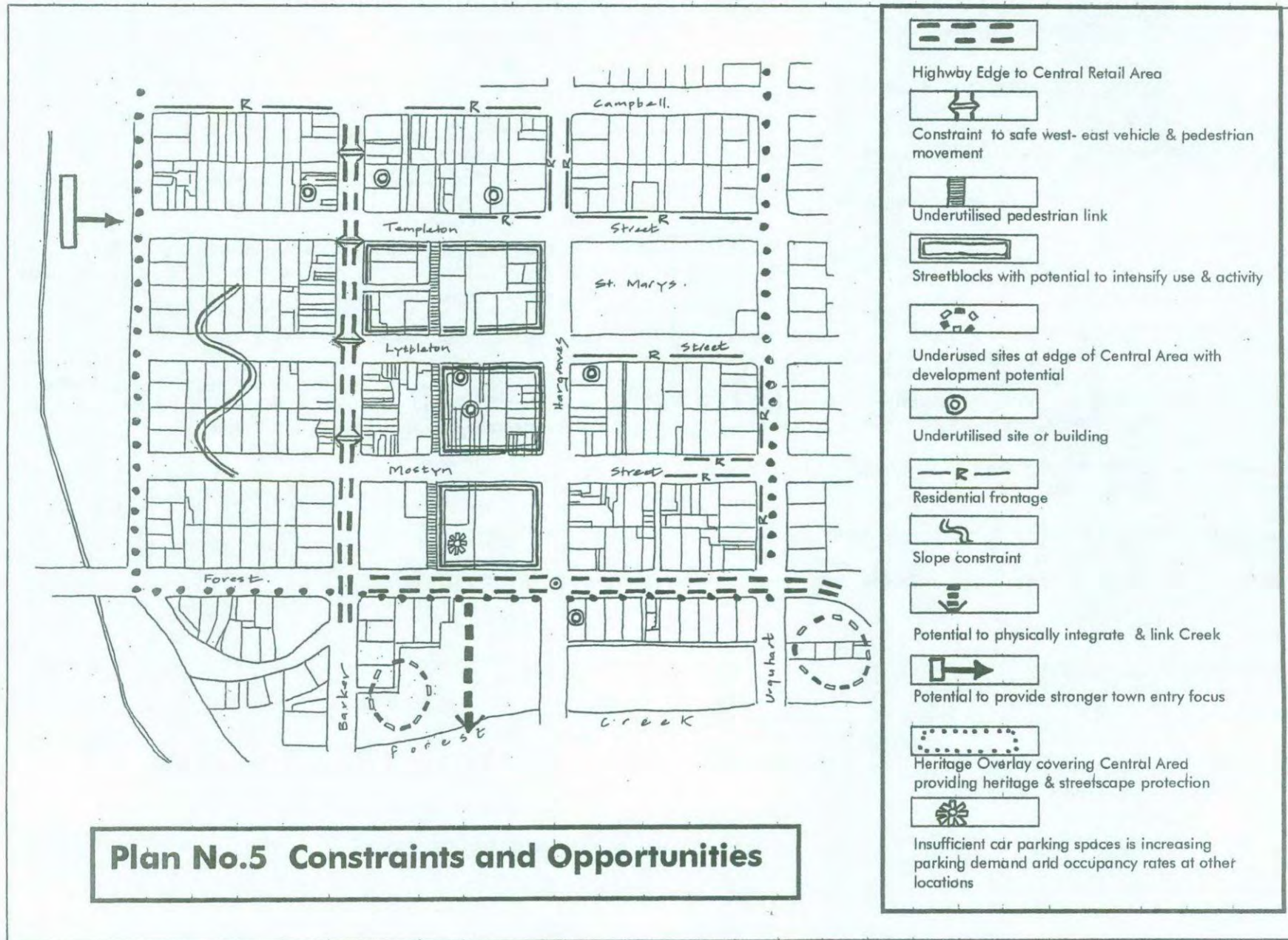
Constraints to consolidation and revitalisation of the Central Area are:

- Heritage integrity, character and form of the Central Area need to be protected and enhanced.
- Forest Street and Barker Streets need to perform roles as state highways.
- Thoroughfare function of Barker Street is creating a physical barrier to safe and convenient movement by vehicles and pedestrians.
- Residential frontages along Campbell, Templeton, Hargraves, Lyttleton and Urquhart Streets need to be retained.
- Slope gradient of the streetblocks west of Barker Street is a severe limitation to further development in this area.
- Car park at the rear of the Market building provides insufficient spaces for demand generated by supermarket and Market Square activities. This is creating parking problems in Mostyn and Hargraves Streets.

Opportunities include:

- Forest Street and Barker Streets form edges to the Central Retail Area.
- Frederick Street is an underutilised pedestrian link. Vistas and view closures can be created along Frederick Street.

- Use of the Market Square Precinct could be intensified. Options such as an outdoor market flanking the Market building should be investigated.
- Several underutilised buildings and sites within the Central Area represent potential opportunities for new uses and development.
- Use of the Civic Precinct could be intensified by encouraging better use of some buildings and sites.
- Underused sites beyond the current edge of the Central Area, south of Forest Street, have development potential for uses and activities that do not require a central location.
- Potential to provide better links to Forest Creek.
- Railway Station town entry at the station and along Templeton Street could be improved by creation of stronger vistas. Tourism retailing and services should be encouraged in this Precinct.



2.7 Availability of Sites

- A preliminary assessment of available sites was undertaken as a background study to this Urban Design Framework.

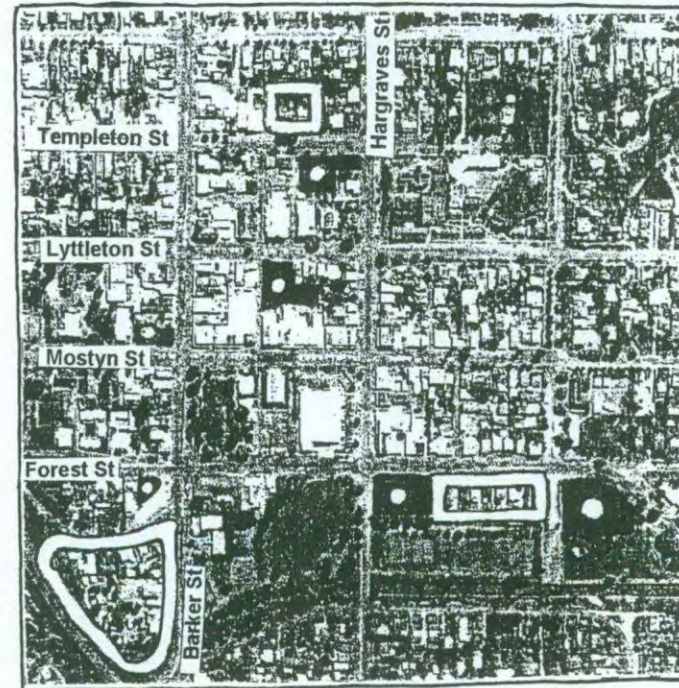
An audit of land and buildings within the Central Area indicates that approximately 2.0 hectares, involving 6 sites, have potential for future consolidation.



Consolidation of the Central Area based on existing urban form can occur through development of some vacant sites. These sites may include smaller and incidental spaces, redevelopment of key sites that do not have any special heritage value and sites where a retail or service use would be a more beneficial use.

Consolidation by re-use of buildings has been identified in other reports and include buildings such as the former School of Mines building in Lyttleton Street, the above ground floor of the Post Office, Peinwa building, Country Fire Authority Station and house, and the Continuing Education building in Templeton Street. The Civic Precinct Action Plan (2000) examines future use options for buildings within the precinct. Generally the Action Plan raises opportunities for transferral of existing uses within the Precinct and does not identify options for any additional commercial use of buildings within the Precinct.

Options for expansion are limited. Expansion to the east along Lyttleton and Mostyn Streets is constrained by residential properties. Expansion to the west along the same streets is constrained by slope, and church and other heritage buildings. Expansion to the north is constrained, though to a lesser extent, by residential buildings situated on the northern side of Templeton Street.

An audit of land and buildings within the Central Area identified approximately 3 hectares of land at the edge of the Central Area that may be suitable for expansion purposes.



-  Priority Development/Redevelopment Sites - Retail/Commercial
-  Longer Term Development/Redevelopment Sites

Plan No.6 Consolidation And Expansion Sites

3. URBAN DESIGN FRAMEWORK

The urban design framework provides a longer term vision for the Central Area. Its purpose is to provide a coordinating strategy for future retail and commercial growth. The framework identifies opportunities in anticipation of market demand and recommends preferred land use and design outcomes at key sites. Access strategies for cars and pedestrians are central to the urban design framework. Initiatives that can be pursued or taken up in the short term, say within five years, are also identified.

The main elements of the framework are:

- Improving vehicle and pedestrian access and links
- Providing development opportunities for business
- Enhancing heritage and landmark sites
- Establishing a precinct framework for action plans
- Identifying key sites for new development/redevelopment
- Creating public space areas integrated with new developments
- Providing opportunities for urban art

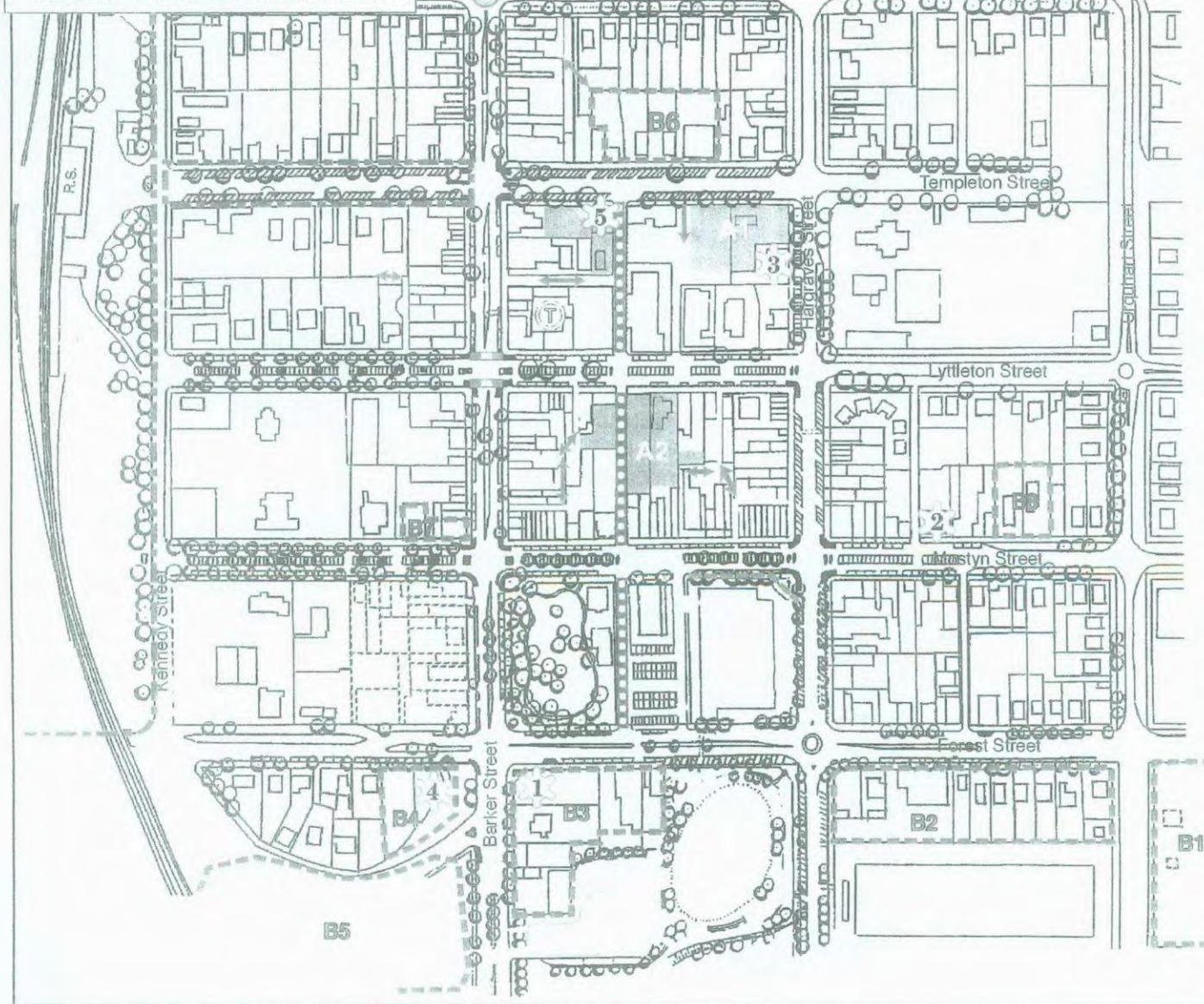
These elements are covered in detail in Section 3 and 4 of this report. The Urban Design Framework is illustrated in Plan No. 7.












3.1 Urban Design Objectives

The objectives provide overall guidance for the consolidation, expansion and revitalisation of the Castlemaine Central Area.

1. Continue to develop the historical commercial Central Area as the primary focus for retail, business, and cultural activities.
2. Recognise that the conservation and enhancement of the Central Area's urban form and heritage character is the most effective form of treatment and revitalisation.
3. Encourage consolidation within the Central Area by conservation and enhancement of heritage buildings and streetscapes, sensitive redevelopment of selected sites and re-use of existing buildings.
4. Encourage limited and planned expansion of the Central Area to the south utilising the Forest Street frontage.
5. Maximise opportunities to establish a public access system that links existing and proposed activity areas and sites.
6. Identify strategic nodes for car parking areas based on the location of high demand retail, business and community uses.
7. Ensure that an adequate supply of car parking is provided with new developments either through on-site provision or by contribution to off site car parking.
8. Encourage physical and retail balance by establishing a precinct framework that identifies preferred uses for defined precincts.
9. Recognise the strategic importance and urban design opportunities of the highways (Barker Street and Forest Street) that pass through the Central Area.

**PLAN NO.7 : CASTLEMAINE
TOWN CENTRE URBAN
DESIGN FRAMEWORK PLAN**



-  **Key Development Areas & Sites** in Central Core. Apply Design & Development Overlay to identified sites. Includes provision for car parking & public space. Refer to Section 4 of Urban Design Framework (UDF) Report for detail on specific sites.
-  **Potential Development/ Redevelopment Sites & Areas.** Apply Design & Development Overlay to identified areas/sites. Includes provision for car parking & public space. Refer to Section 4 of UDF Report for detail.
 - B1, B5: Retail/Residential mix
 - B2, B3: Retail/Office mix
 - B4, B6-8: Retail
-  **Implement Recommendations** of Castlemaine Railway Precinct Master Plan.
-  **Implement "Link to Town Centre" Recommendations.** (Castlemaine Railway Precinct Master Plan)
 - Encourage Residential Mix** with retail & commercial in future development/redevelopment proposals.
-  **Proposed Pedestrian Access Spine**
Improve pedestrian and visual amenity.
- Minor Pedestrian Access**
Make connections through street blocks.
-  **Pedestrian Connection Required or Reinforced**
-  **Relocate Traffic Signals** to Lytleton Street intersection.
-  **Proposed Roundabout**
Marking entry to town centre and to calm traffic through Barker St. precinct.
-  **Change in Use of Premises Recommended.** Refer to Section 4 of UDF Report.
-  **Relocate Telecommunications Building** - create Post Office Plaza
- Youth Activity Area**
-  **Remove Physical Barriers**
 - make open to street
 - improve amenity of public space areas

3.2 Use and Activity

A major shift in activity and geographical focus has occurred in recent years. The location of the IGA supermarket in Mostyn Street and the restoration and re-use of the Market building have influenced other stores, that enjoy frequent shopper visits, to re-locate to Mostyn Street (newsagency, chemist and bank).

This shift of focus to Mostyn Street has had an impact on Barker Street. The Barker Street precinct has experienced several tenancy changes in recent years, and some of its key stores have relocated to Mostyn Street.

One aim of Urban Design Framework is to provide opportunities for the location of a major store or stores at the north end of the Central Area which would act as an "anchor" for existing and new retail shops in Barker Street and Templeton Street.

Preferred uses along major street frontages in the Central Area are shown on Plan No. 8.

Active uses

Active uses are uses that generally encourage public interaction and make a contribution to the lively character of a street.

Uses that are considered to be active include:

- Retail shops
- Cafes and restaurants
- Education and health services

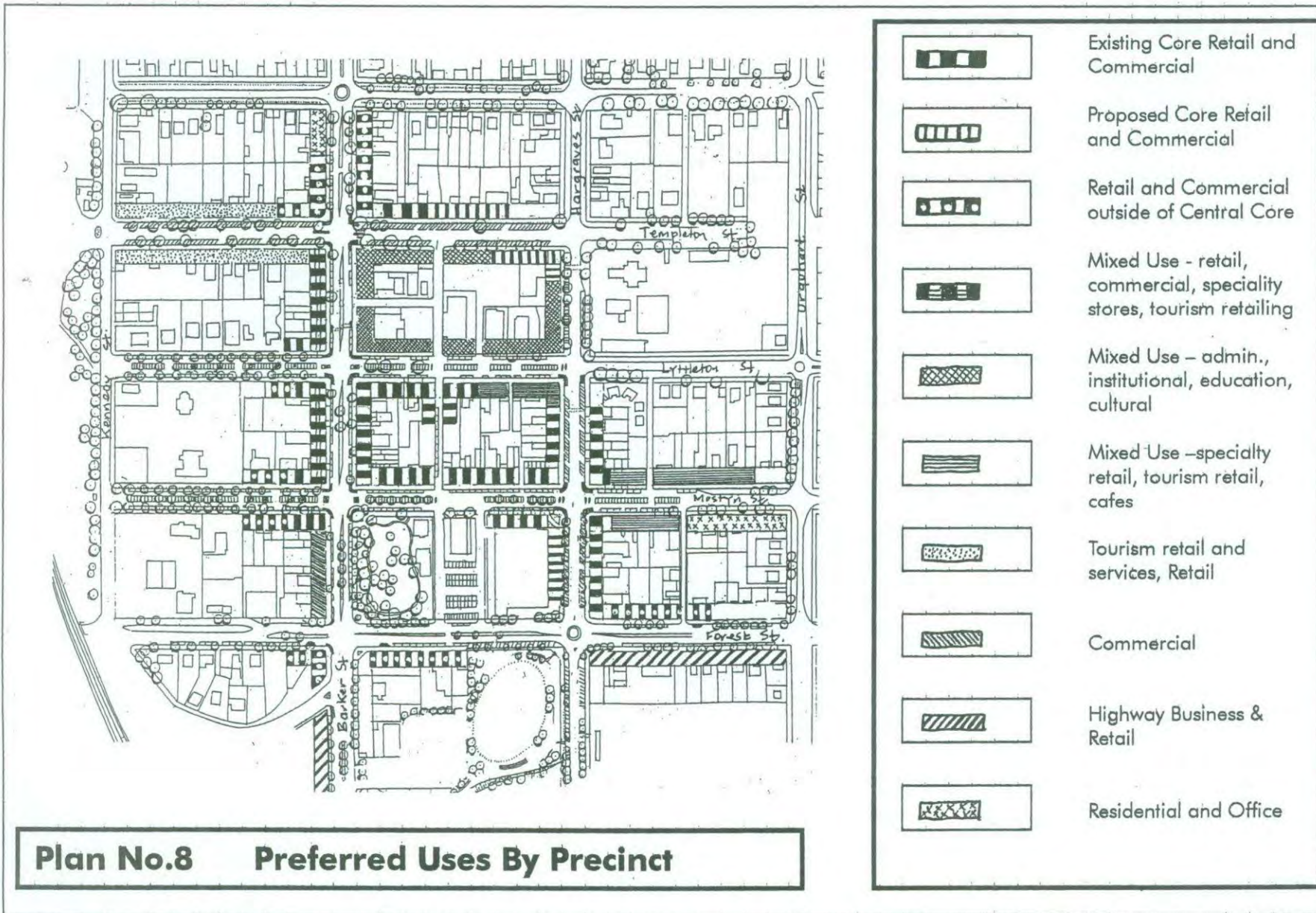
Active type uses are proposed for the new development sites in Hargraves, Lyttleton, Barker and Templeton Streets. The sites are shown on Plan No.7,

New buildings at these sites should be built to the street frontage to foster street activity.

The existing use of some buildings do not complement the character or atmosphere of its precinct. Change of use in these cases should be encouraged. The location of these buildings is shown on Plan No.7.

Objectives

- Provide a precinct framework approach to encourage balance in retail and commercial activity by identifying preferred uses for different precincts.
- Encourage retail and business stores that depend on a central location to locate within the Central Core precinct.
- Encourage mixed use (retail, tourism retailing, specialty type stores, offices) along the west side of Barker Street and in Templeton Street from Barker Street to Kennedy Street.
- Encourage uses that benefit from highway exposure but are not dependent on a central location to establish within the Barker Street southern entry precinct.
- Encourage uses to locate in Mostyn Street East End Precinct that complement the existing active character of the precinct.
- Encourage active ground floor uses within the Central Core, Barker Street West and Mostyn Street East End Precinct.
- Encourage residential living within the Central Area eg. shop top housing.



Activity Flows

Activity flows and foci are shown on Plan No. 9. Major pedestrian flows are concentrated along Mostyn Street between Hargraves and Barker Street and along Barker Street between Mostyn Street and the Post Office corner in Lyttleton Street.

Major activity foci include the IGA supermarket and adjacent Market building, Mostyn Street East End, Theatre Royal, Post Office, Library, Town Hall, School of Mines, Riteway Supermarket and the railway station.

New Activity Areas

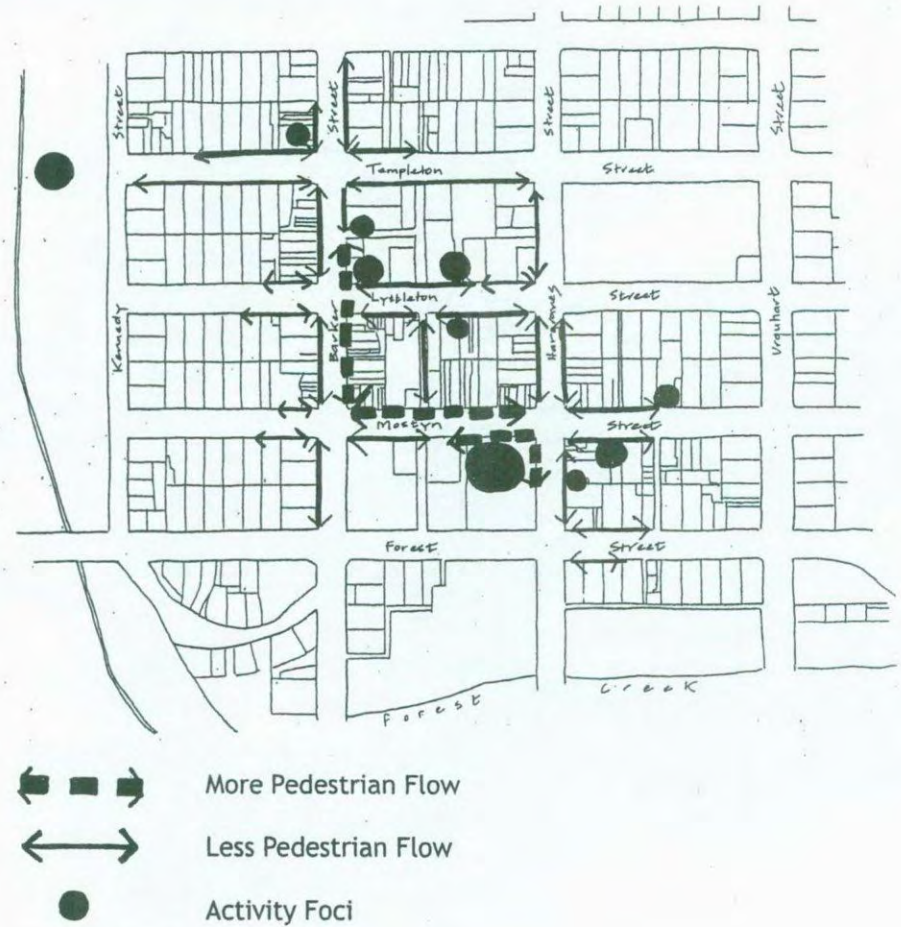
New activity areas are proposed within the following precincts:

- Civic Precinct
- Forest Creek Precinct

Proposals for these precincts are discussed in Section 4, Precinct Framework.

Central Living

Residential living should be encouraged at the above ground floor level of buildings. Opportunities are available in Mostyn, Hargraves, Lyttleton and Barker Streets.



Plan No. 9 Activity Flows and Foci

3.3 Access and Links

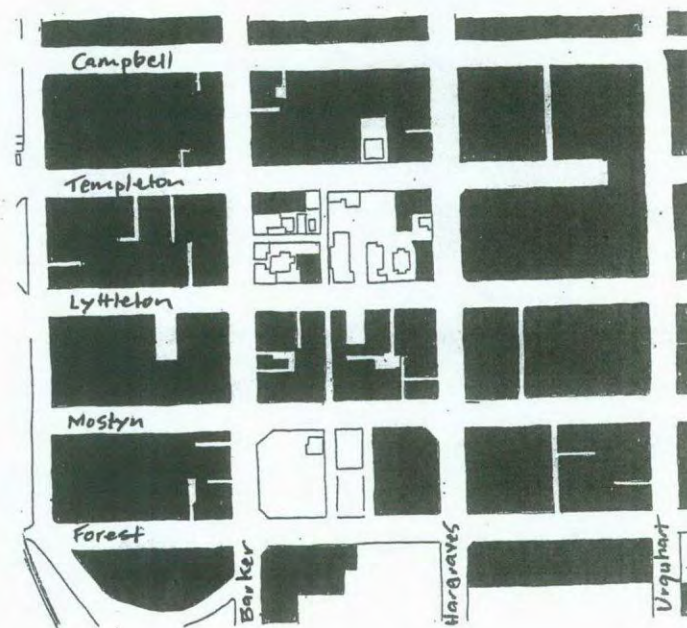
Pedestrian Access

Plan No10 illustrates the on-ground relationship between public and private land. It also shows the location of public space areas and how they are connected by public access ways including roads and street pavements, laneways and public space areas. The plan highlights the following characteristics:

- Mid block connections running north and south between Forest and Templeton Streets along Frederick Lane.
- Major public space areas (Western Reserve, Victory Park, and Civic Precinct) connected by the mid block links.
- A network of laneways that connect with the major streets.
- A network of laneways permeating the Central street block (Mostyn, Barker, Lyttleton and Hargraves Streets) providing connections to all major streets.

The Access Framework Plan (Plan No.11 Access Links and Public Spaces) reinforces and maximises opportunities of the existing underutilised pedestrian access network.

There are many opportunities to create interesting ways of getting places in the Castlemaine Central Area. In many cases these accessways can be linked and designed as public places that are integrated with new developments. They can be a positive feature of a new development. Their use can make an important contribution to creating physical balance in the town centre.



Plan No. 10 Existing Public Space

Vehicle Access

Vehicle movements through and within the Central Area are concentrated in Barker Street and Mostyn, Lyttleton, Hargraves Streets between Barker and Hargraves Street.

The Midland Highway (Barker Street) and Pyrenees Highway (Forest Street) provide an edge to the Central Retail Area. These sections of highways provide for both through and local traffic. Traffic counts conducted by VicRoads indicate the following traffic volumes at entry and exit points to and from the Central Area:

Average Daily Traffic: (Two Way): Midland Highway–Barker Street

Downes St before rail underpass)	4780 (1993) 5180 (1998) 6855 (2004)
50 metres north of Templeton St	8187 (1992)
North of Greenhill Ave	7400 (1992)

Average Daily Traffic (Two Way) : Pyrenees Highway–Forest Street

Happy Valley Road	7370 (1993) 6535 (1999)
Gaffney St	4550 (1993)
400m east of Castle/Maldon Road	4137 (2004)

Traffic safety for vehicles and pedestrians is an issue in Barker Street. Potential conflict points along Barker Street are at intersections with Lyttleton, Templeton and Mostyn Streets

Movements across and into Hargraves Street at the intersection with Mostyn Street are becoming more difficult.

Car Parking

Availability of car parking spaces is becoming an issue particularly within the central retail area along Mostyn, Lyttleton, Hargraves and Barker Streets. Council is preparing a parking study for the Central Area.

A car parking study was undertaken in 1997 just prior to the construction of the new supermarket in Mostyn Street. The study objectives included “ determining the likely effects of the proposed supermarket on the availability of existing car parking spaces.”

The Castlemaine Town Centre Parking Study (Andrew O’Brien and Associates 1997) concluded that high parking demand and turnover occurs in the following streets :

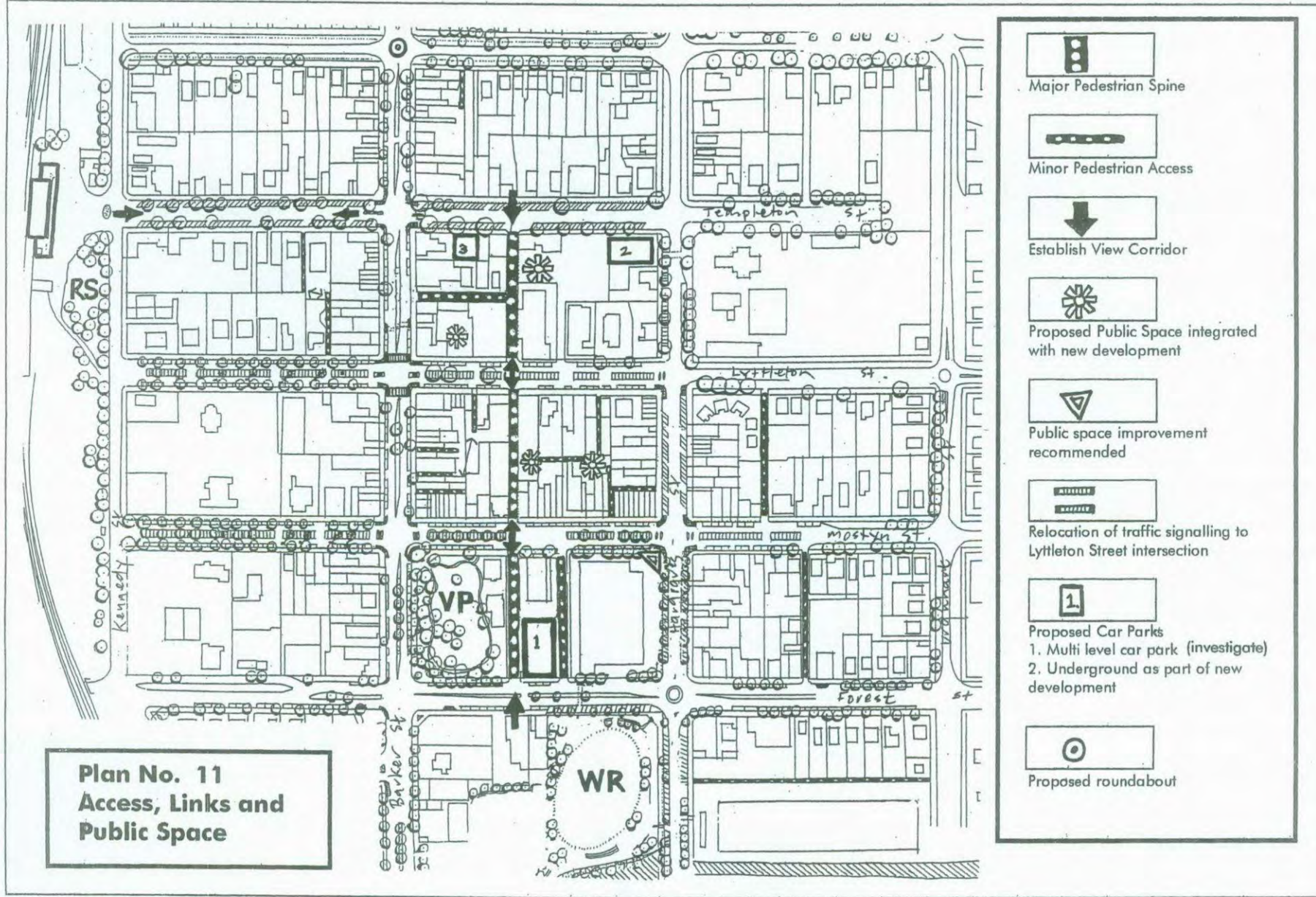
- * Within the Central Core in Mostyn, Lyttleton and Hargraves Streets. Barker Street.
- * Templeton Street West near to Barker Street
- * Centre of the road parking in Lyttleton and Mostyn Streets west of Barker Street

Results of the parking demand surveys are summarised and illustrated on Plan No. 12.

Since the establishment of the supermarket in Mostyn Street the demand for car parking spaces has risen in Hargraves Street between Forest and Lyttleton Streets.

Demand has also risen in Mostyn Street East due to active uses in the precinct and the proximity to the supermarket.

The Castlemaine Town Centre Parking Study lists the following issues that were raised in discussions between the traffic consultant and the Council at the time of the parking study.



-  Major Pedestrian Spine
-  Minor Pedestrian Access
-  Establish View Corridor
-  Proposed Public Space integrated with new development
-  Public space improvement recommended
-  Relocation of traffic signalling to Lytleton Street intersection
-  Proposed Car Parks
 1. Multi level car park (investigate)
 2. Underground as part of new development
-  Proposed roundabout

- Local trader concerns about availability of nearby long term parking facilities following development of the supermarket.
- Mostyn Street East Traders concerns about lack of parking for staff and customers
- Desire to develop additional parking opportunities in the core retail area.

The location of uses and activities that generate high demand for car parking are shown on Plan No.12. Strategic location for off street car parking are also indicated on this Plan.

Off Street Parking Proposals

Civic Precinct

The north east corner of the streetblock has been identified as a potential redevelopment site. (Refer to Section 4.1) Modified ground levels provide an opportunity for underground car parking as part of new development for the site.

Lyttleton Street / Frederick Street

A mid block site has been identified for potential redevelopment (refer to Section 4.1 and Plan No. 16). The site includes the existing car park, Police Station (demolition proposed) and possibly some land at the rear of properties fronting Lyttleton and Hargraves Streets.

An off street car park to replace the existing car park plus provision of additional spaces would be required as part of the overall redevelopment.



Fig 4: Existing carpark at rear of Market Building identified as possible site for multi-level carpark. Design of multi-level car park should be subject of major design competition and should be designed so that it can be adapted for use as a building at a later date.

Access Objectives

- Make better use of existing laneways and right of ways as a means to create though block links.
- Ensure that accessways and public space areas are well designed and interesting spaces that lead to places of activity and interest.
- Identify opportunities for new retail and commercial buildings adjacent to the access system.
- Provide locations for car parking that are of minimal visual impact and which do not require the removal of valuable components of the existing built form.
- Encourage the location of car parks on underutilised land at the rear of retail and business premises.

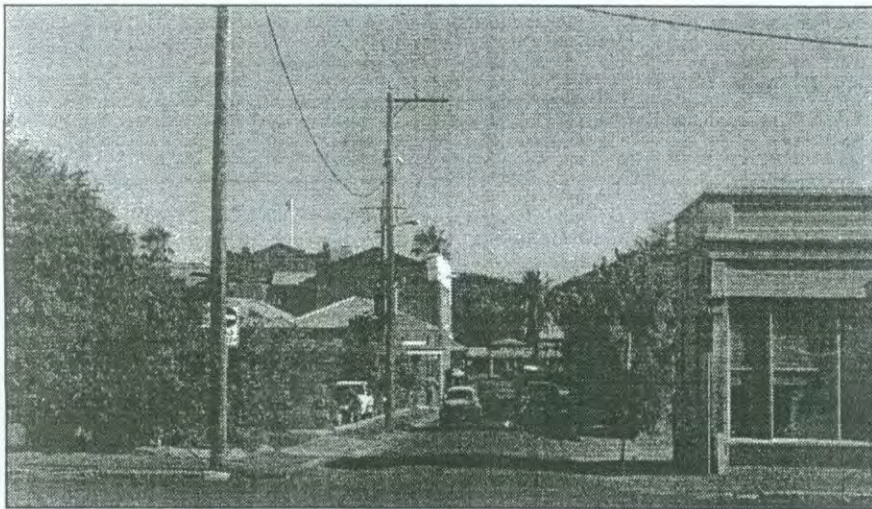


Fig 5: Frederick Street has potential to become major pedestrian spine connecting new development sites.

Strategies

- Encourage pedestrian movement along the access system identified in the Access Framework by plantings, pavement design and appropriate signage.
- Establish Frederick Lane as a pedestrian spine linking the Market Square to Lyttleton Street, the Civic Precinct and Templeton Street. It is recommended that existing parallel parking be removed.
- Investigate potential for a multi-level car park (underground, ground and upper levels) at the rear of the Market building with entry and exit points from Forest Street.
- Incorporate an underground car park within the new development proposed for the north east corner of Templeton and Hargraves Sts.
- Relocate traffic signalling to the intersection of Barker and Lyttleton Streets with traffic signal phasing giving priority to vehicles crossing and turning into Barker Street and for pedestrians crossing Barker Street.
- Establish roundabout at the intersection of Campbell and Barker Streets as a means to calm traffic through Barker Street and to create an entry statement to the Central Area.

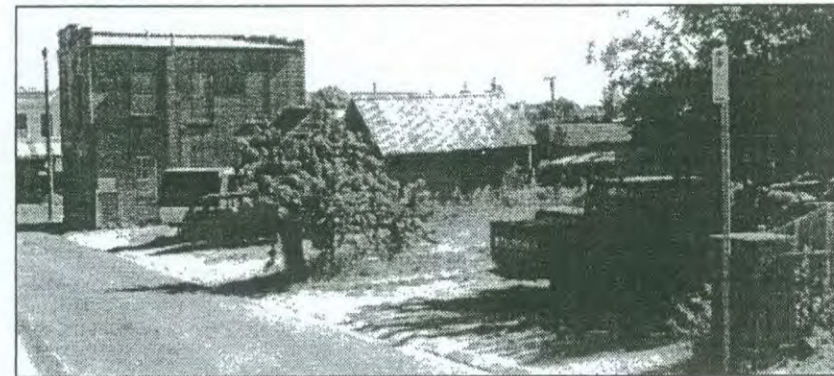
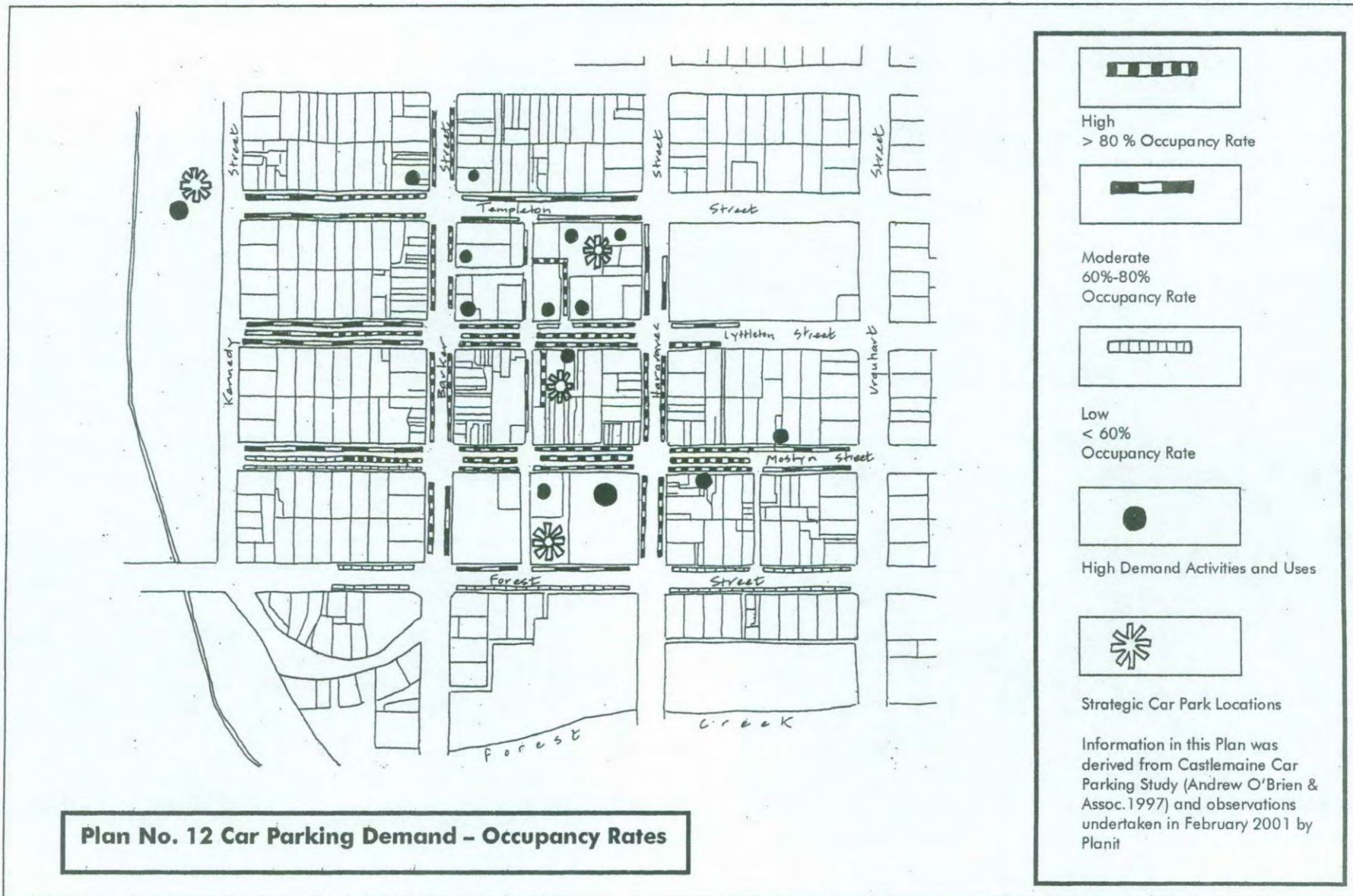
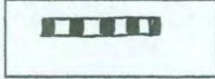
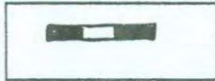



Fig 6: Potential infill site located in Lyttleton Place





Plan No. 12 Car Parking Demand - Occupancy Rates


 High
 > 80 % Occupancy Rate


 Moderate
 60%-80%
 Occupancy Rate


 Low
 < 60%
 Occupancy Rate


 High Demand Activities and Uses


 Strategic Car Park Locations

Information in this Plan was derived from Castlemaine Car Parking Study (Andrew O'Brien & Assoc. 1997) and observations undertaken in February 2001 by Planit

3.4 New Development Opportunities

Development opportunities for the Central Area fall into two categories:

Consolidation Sites: sites recommended for future development including both vacant sites and sites for possible redevelopment.

Expansion Sites - sites situated at the edge of the existing Central Area.

Consolidation of the Central Area based on existing urban form can occur through development of some vacant sites. These sites include smaller and incidental spaces, redevelopment of key location sites that do not have any special heritage value. They also include locations where a retail or service use would be a better use of the site.

The overriding concern with consolidation is heritage preservation. The heritage character and integrity of the streetscapes, groupings of buildings and individual buildings must be protected against unsympathetic adjacent developments.

There are constraints to expansion of the Central Area. These constraints are discussed in Section 2.6. Expansion is limited to the south side of Forest Street with some redevelopment potential of existing properties. Key sites have been identified for new retail and community facilities adjacent to Urquhart Street and near Barker Street to the west of Western Reserve. Uses that do not depend on a central location should be encouraged to locate at these sites. Details about the future use of these sites are discussed in Section 4.2.

Objectives

- Encourage the removal of unsympathetic buildings and replacement with buildings that are more appropriate to the heritage and streetscape character.

- Encourage better utilisation of existing retail and commercial buildings in the Central Area.
- Ensure that the form of new development is appropriate to the general character of the Central Area and to the specific site.
- Encourage the future use of underutilised sites and buildings and indicate appropriate uses and forms of development for these sites.
- Ensure that adequate provision is made for vehicle and pedestrian access, and car parking within new retail and commercial developments.
- Ensure that consultation with Council's Heritage Advisor is undertaken at the pre-application stage to establish the required design outcomes for a site.



Fig 7: Potential longer term re-development site

Strategies

- Encourage new retail development to locate at the development sites indicated on the Urban Design Framework Plan.
- Priority sites for new retail and commercial development identified on the Urban Design Framework Plan are:
 - North east corner of Templeton and Hargraves Street currently occupied by the Community House and vacant land at the rear of the School of Mines building.
 - South east corner of Forest and Wheeler Streets currently occupied by Hunt and Lobb P/L.
 - Lyttleton Street and Frederick Lane site comprising the existing car park, and the Police Station which is recommended for removal. There must be no net loss of parking.
 - Re-use of the Castlemaine Bus Lines building situated at the corner of Barker and Forest Streets.
 - Redevelopment of the site located at the corner of Forest and Barker Streets currently occupied by Scullies Car Sales.
- Prepare concept plans for new development sites to achieve integrated use and development of sites.

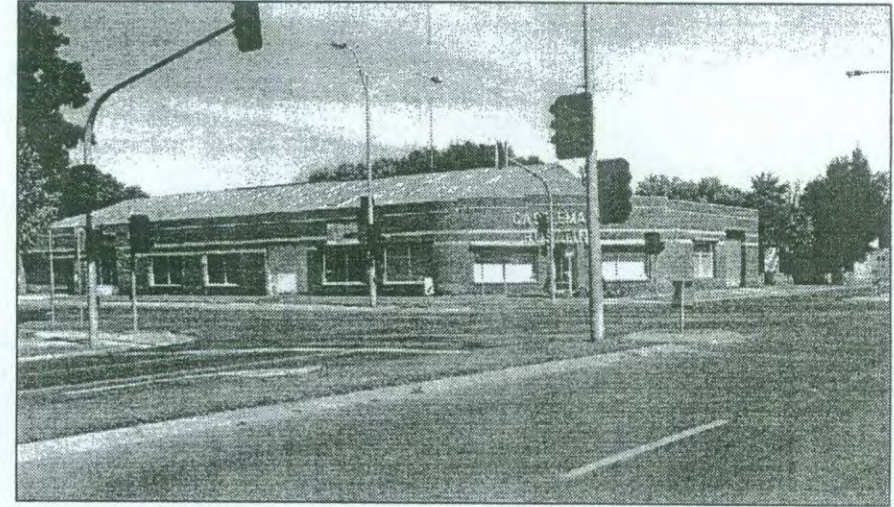


Fig 8: Castlemaine Bus Line's building: A change of use of the building would be more beneficial to central area.

3.5 Heritage and Landmark Sites

The heritage character of the Central Area is of State and national importance. Conservation and enhancement of the heritage buildings, sites and streetscapes is of paramount importance to any proposals for new development or redevelopment within the Central Area.

The city's historic and architectural significance is exemplified by the numerous listings of buildings and sites by the Historic Buildings Council, the National Estate and the National Trust. The Mount Alexander Planning Scheme provides heritage protection for the entire Central Area and for numerous buildings and sites which have individual listing.

Listed heritage buildings, buildings that make important contributions to heritage streetscapes and obtrusive later buildings are shown on Plan No. 13.

Objectives

- Encourage conservation and enhancement of all buildings that contribute to the Central Area's historic character.
- Undertake a heritage study for the Central Area to review the Architectural and Heritage Study, 1979, and provide a historical context for all new development including capital works proposed in the public realm.
- Provide guidelines for the integration of new development into the existing streetscape.

Strategies

- Make application to Heritage Victoria for funding to undertake a major review of the "City of Castlemaine Architectural and Heritage Study", (1979).
- Make application to the Department of Infrastructure's Pride of Place program for funding to produce design guidelines for new development and redevelopment sites identified in the Urban Design Framework.

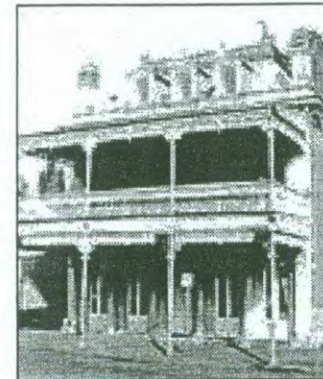
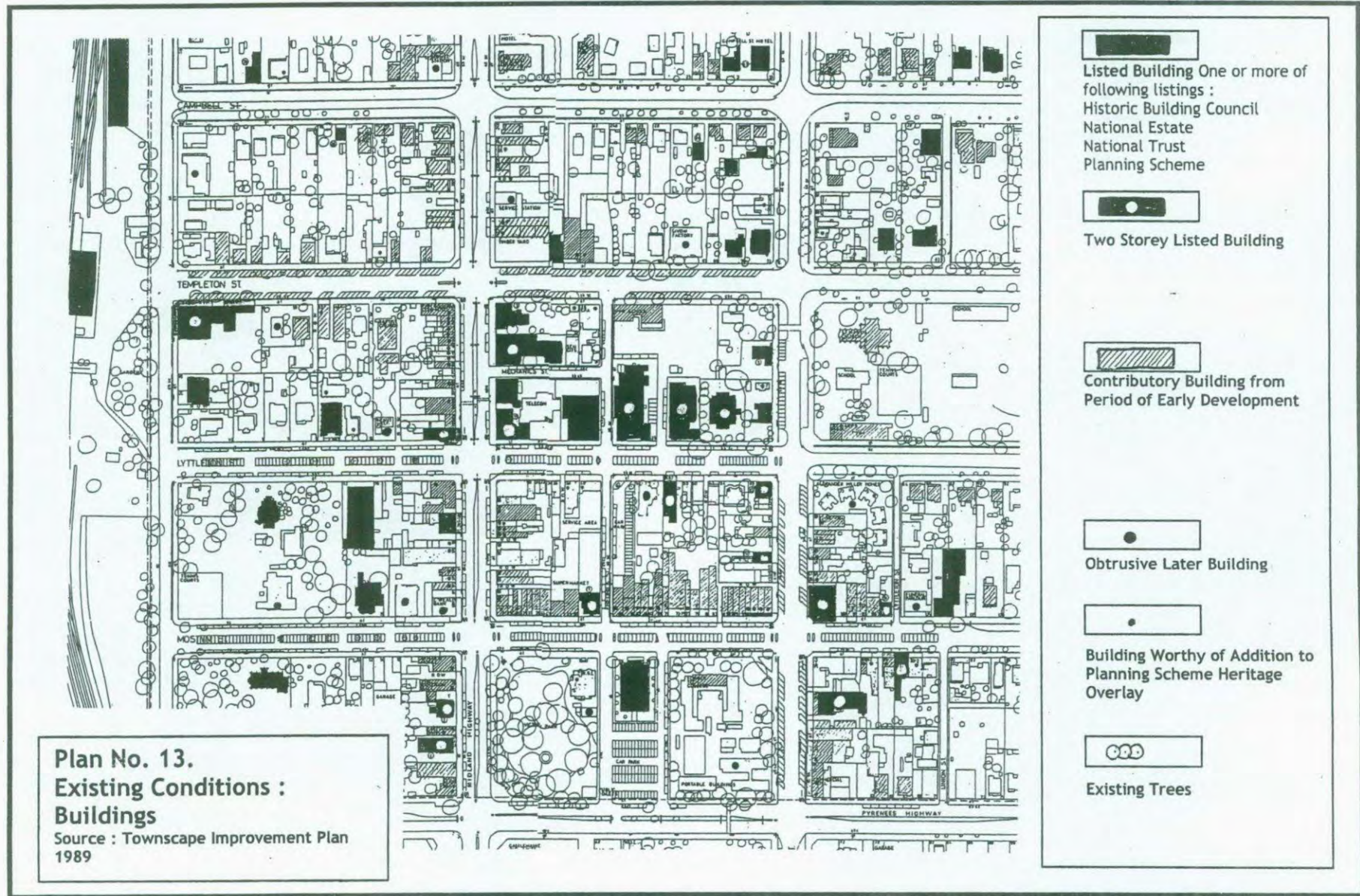


Fig 9: Heritage landmark building - Beck's Imperial Hotel



4. PRECINCT FRAMEWORK

The following precincts have been identified for the purpose of analysis and the formulation of urban design objectives and strategies.

- Central Core
- Forest Street (south side)
- Barker Street Southern Entry (west side)
- Mostyn Street - East End
- Barker Street Corridor
- Barker Street-Railway Station Precinct.

The precincts are shown on Plan No. 14.

Objectives and strategies for each precinct are discussed below.



Precincts

- | | |
|--------------------------------------|-----------------------------|
| 1. Central Core | 2. Forest St/Forest Creek |
| 3. Forest St Entrance | 4. Barker St Southern Entry |
| 5. Mostyn St East End | 6. Barker Street |
| 6. Railway Station/Templeton St West | |

Plan No. 14 Central Area Precincts

4.1 Central Core

The Central Core is defined as the four street blocks north of Forest Street between Hargraves and Barker Streets.

Consolidation has the greatest potential within the Central Core for the following reasons:

- Opportunities for increased pedestrian access along Frederick Lane and its southern extension through the Market Square Precinct.
- Continuous access permeates three of the street blocks.
- A network of laneways, most of which are not used, provide connections through the street blocks to Barker, Mostyn, Lyttleton and Hargraves Streets.
- The major activity focus of the Central Area is located within the Central Core in Mostyn Street.
- Undeveloped land exists within three of the street blocks. Comprehensive and well-designed development of these sites could take place providing for a mix of retail, business, car parking and designed public places.

Forest to Mostyn Street Block

Known as the Market Square Precinct, the street block accommodates the IGA Supermarket, Market Building, car parking and Victory Park.

The Market building is the symbolic focus of the Central Area.

Mostyn Street is the core retailing strip in the Central Area.

The bulk and scale of the supermarket has significantly altered the Forest Street streetscape.

The shortage of available car parking spaces in the southern end of the Central Area is partly a consequence of the parking demand generated from the supermarket.

Car parking along the frontage of a main street such as Hargraves Street is an example of poor urban design. Active use of this frontage would be more beneficial to street life in Hargraves Street.

Sites within the streetblock that have been identified for new use and development are shown on the Urban Design Framework Plan. See Site A3 on Plan No. 7.

Objectives

- Maintain the Market building as the geographic and symbolic focus for the Central Area.
- Reinforce the activity focus of the Market Square Precinct.

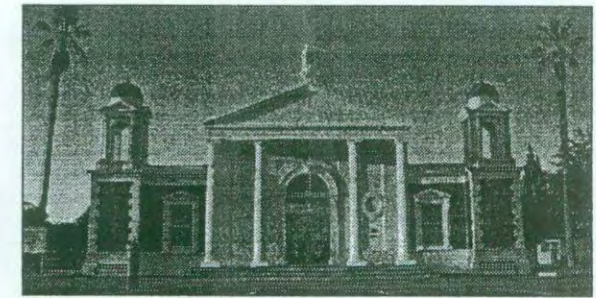


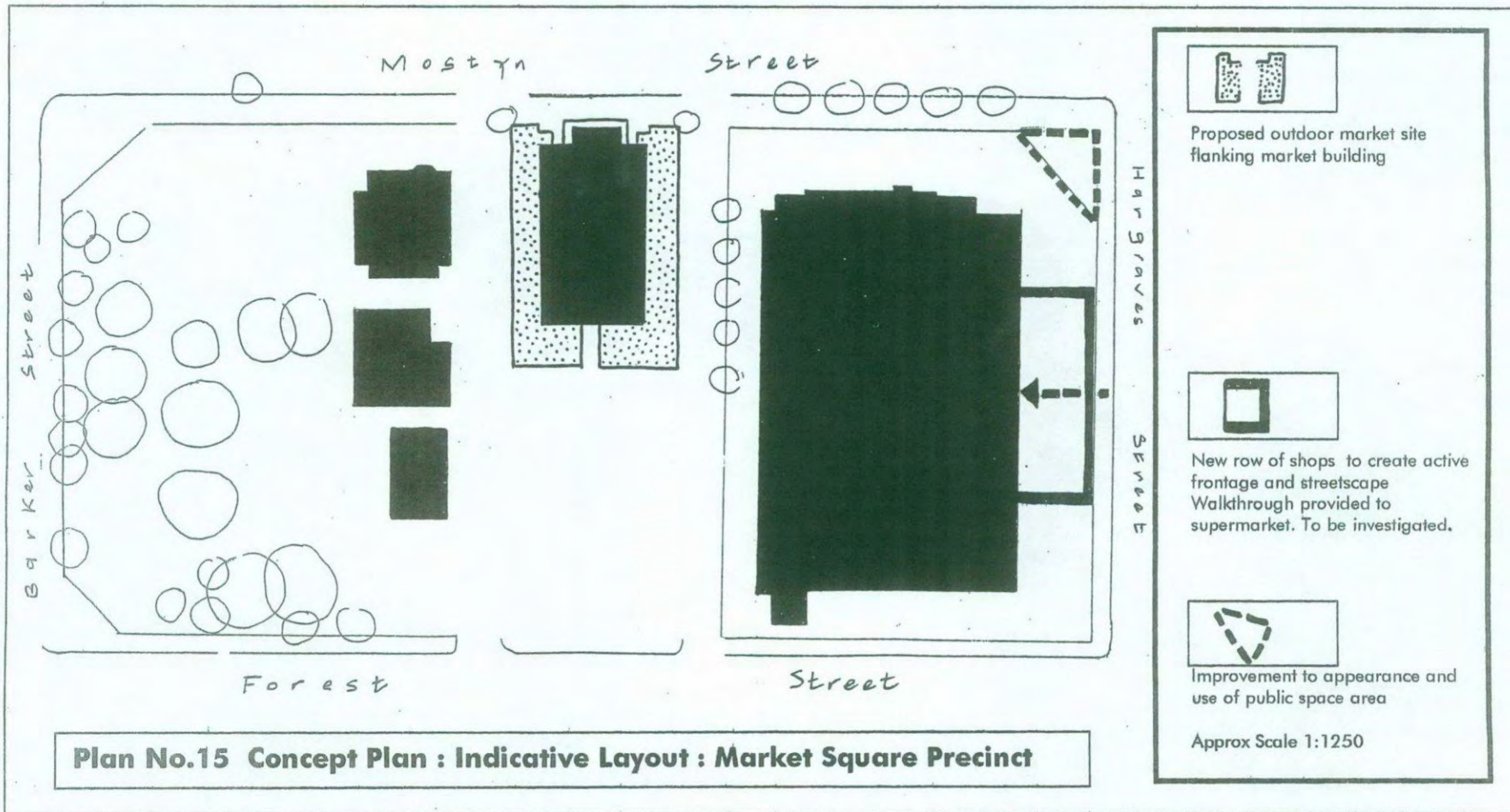
Fig 10: Castlemaine Market Building - Mostyn Street
Castlemaine's symbolic focus

Strategies

- Develop the area flanking the Market building as an outdoor market.
- Investigate the potential for the construction of a row of shops along the Hargraves Street frontage at the site currently occupied by a car park. Direct access to the supermarket from the shops would need to be provided by way of an arcade or walk-throughs.

Concept Plan

See Plan No. 15 (Concept Plan : Indicative Layout Market Square Precinct) which illustrates proposed building massing and the location of the proposed car park and outdoor market.



Plan No.15 Concept Plan : Indicative Layout : Market Square Precinct



Proposed outdoor market site flanking market building



New row of shops to create active frontage and streetscape Walkthrough provided to supermarket. To be investigated.



Improvement to appearance and use of public space area

Approx Scale 1:1250

Mostyn-Lyttleton Street Block

The central street block has major retail frontages to Mostyn Street and Barker Street. A major development opportunity is available within the street block at the frontages of Lyttleton Street and Frederick Lane.

The site also includes an infill site situated on the west side of Frederick Lane at the rear of the Crazy Prices store. This small site was previously a loading area for the former supermarket.

The above sites are shown on Plan No 7 Urban Design Framework Plan. See Area A2.

Objectives

- Facilitate comprehensive redevelopment of the site identified on the Urban Design Framework Plan. (Site A2)
- Ensure that new buildings contribute to the form and character of adjacent buildings, sites and streetscapes.
- Ensure that redevelopment does not lead to a net loss of car parking.

Strategies

- Demolition of the Police Station building (a late obtrusive addition to the streetscape), to allow for the

construction of a one or two storey building. Active use at ground level is encouraged.

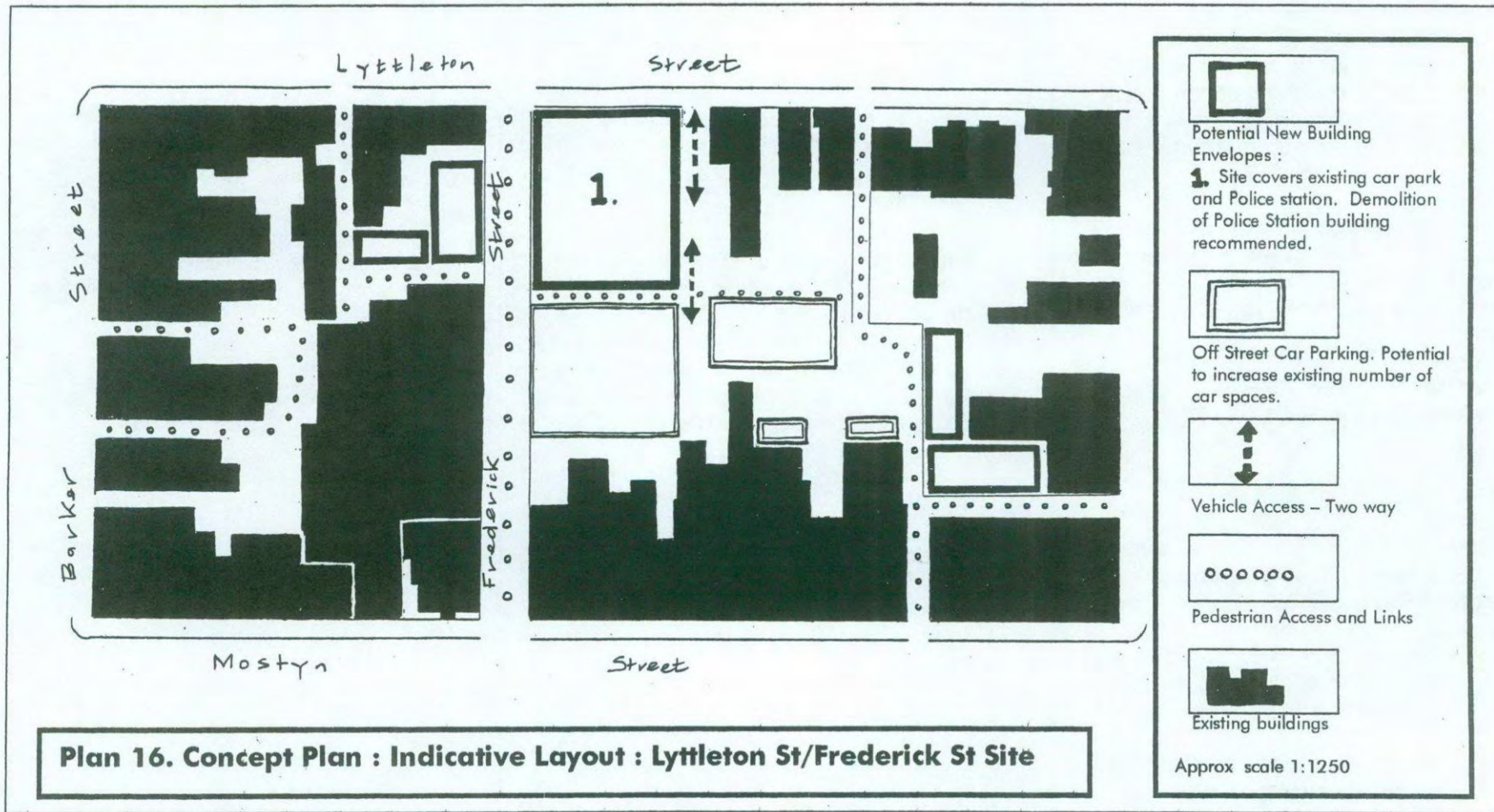
- Prepare a design concept for the site showing building massing; site layout, off street car parking, landscaping and public space areas.
- Encourage continuous building lines along Lyttleton Street by siting new buildings at the street boundary.
- Create through block connections to Mostyn Street, Hargraves Street and Frederick Lane using existing unused right of ways.
- Investigate opportunities for use of the rear of properties, particularly those fronting Lyttleton and Hargraves Streets.
- Encourage through block links to Barker Street and Lyttleton Street using existing right of ways.

Concept Plan

See Plan No. 16 , which illustrates proposed building massing, layout and access.



Fig 11: Potential Development site: Lyttleton Street



Plan 16. Concept Plan : Indicative Layout : Lyttleton St/Frederick St Site

Potential New Building Envelopes :

1. Site covers existing car park and Police station. Demolition of Police Station building recommended.

Off Street Car Parking. Potential to increase existing number of car spaces.

Vehicle Access – Two way

Pedestrian Access and Links

Existing buildings

Approx scale 1:1250

Lyttleton to Templeton Street Block – Civic Precinct

Known as the Civic Precinct, the street block between Lyttleton and Templeton Streets has historically been an administrative block containing institutional buildings and community facilities. The street block contains one of the finest groupings of civic buildings in the State and nation.

The vacant area at the rear of the Town Hall and School of Mines buildings has been identified in this and previous studies as a potential site for future development. The site is owned by Council. The adjacent site at the corner of the street block, occupied by the Community House, is privately owned.

The telecommunications building situated between the Post Office and former Drill Hall building detracts from the splendour of the Post Office. Removal of this building is recommended.

An Action Plan has been prepared for the Civic Precinct. The report titled "Castlemaine Civic Precinct Action Plan" (Projektion 1999) had the following aims:

- clarify the goals of Council and the community in relation to functions and buildings in the Civic Precinct;

- identify the options and their consequences for its long term development

Sites identified for new use and development are shown on the Urban Design Framework Plan. See Plan No.7, Area A1.

Objectives

- Encourage the continued mix in use of buildings that has been a feature of the precinct, eg civic administration, institutional, education, training, cultural, business and residential.
- Provide for retail/commercial development at the north east corner of the street block with building lines at street frontages of Templeton and Hargraves Streets.
- Prepare a design concept showing proposed building massing, vehicle and pedestrian access and location of public space areas.
- Ensure that new buildings and designed spaces contribute to the existing built form character within the street block and the streetscapes along major street frontages.
- Create pedestrian accessways through the site linked to new dedicated public space areas.

- Removal of the cream brick residential at the rear of the CFA building and investigate alternative uses for the site.

Strategies

- Redevelopment of the north east corner currently occupied by the Community House. Relocation options for the Community House may include accommodation within the new development or relocation to another building within the Central Area.
- Provide underground car parking as part of the overall development of the north east corner site of the streetblock.
- Prepare a "Civic Precinct Local Policy " that articulates Council's vision for the Precinct including preferred uses for existing and any new buildings; and vacant sites within the Precinct. The Local Policy will be included in the Planning Scheme by amendment to the Scheme.

- Encourage active uses for the ground floor use (ie. retail store, cafe, and restaurant etc) to add vitality to Templeton Street and increase shopper movements in the northern end of the Central Area.
- Provide for a public space area or town square at the rear of the Town Hall as indicated on Plan No. 17. Shade trees, seating and urban art to be incorporated into design of the town square.
- Negotiate with Telstra for the relocation of the telecommunications building adjacent to the Post Office to provide an open plaza between the Post Office and Former Drill Hall building (indoor swimming pool).
- Encourage relocation of Parks Victoria from the former Police Station building to facilitate a new use of the building that is complimentary to other proposals advanced for the Civic Precinct streetblock.

Concept Plan

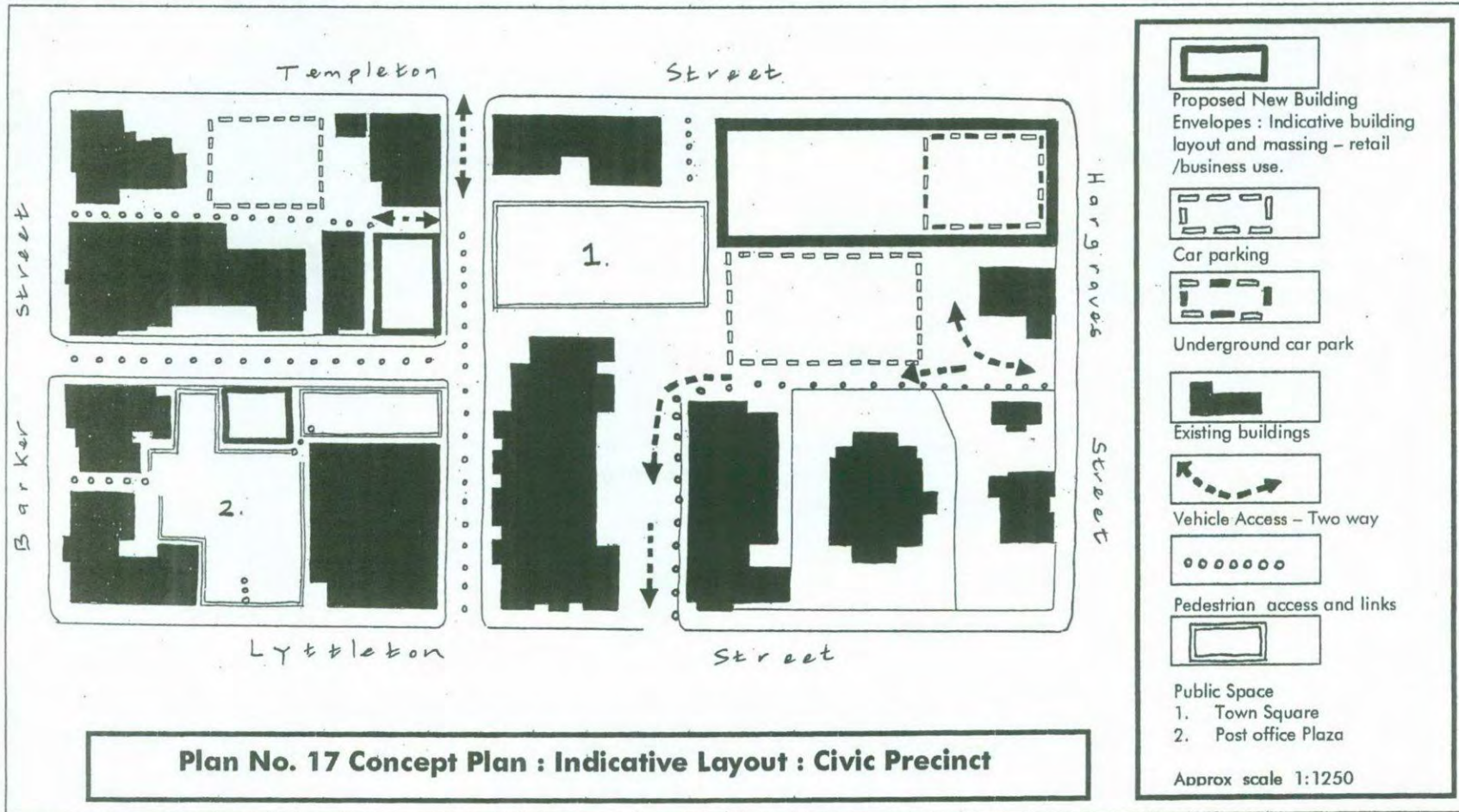
See Plan No. 17, which illustrates proposed building massing, layout and access.



Fig 14: Site at rear of Town Hall identified for mixed use - retail, parking & public space



Fig 15: Relocation of obtrusive telecommunications building would enable creation of Post Office plaza.



Templeton Street

Templeton Street is the northern edge of the Central Core. New retail development along Templeton Street would help to anchor the northern end of the Central Area. With the gravitation of shopper activity to Mostyn Street some commercial impetus is needed in Templeton Street.

A site at the end of the main proposed pedestrian spine (Frederick Street) has been selected as having longer-term development potential. The site covers four properties; Cassidy's warehouse (cream brick building) the Council owned Peinwa building and the two intervening residential properties. These buildings do not possess any special heritage significance.

Preferred uses for the site include a mixed goods store.

A pedestrian connection could be made through to the right of way running off Campbell Street at the rear of shops fronting Barker Street.

The site is identified as a potential redevelopment site on Plan No. 7. See Site B5.

Objectives

- Retain the northern edge of the Central Area at the northern side of Templeton Street.
- Provide retail/commercial opportunities along Templeton Street as a northern anchor to activity generators in Mostyn Street ie IGA supermarket and Market building.

Strategies

- Apply a Design and Development Plan Overlay to the site on the northern side of Templeton Street identified on the Urban Design Framework Plan.
- Investigate potential for re-use of CFA Station in conjunction with adjacent sites to south and east.

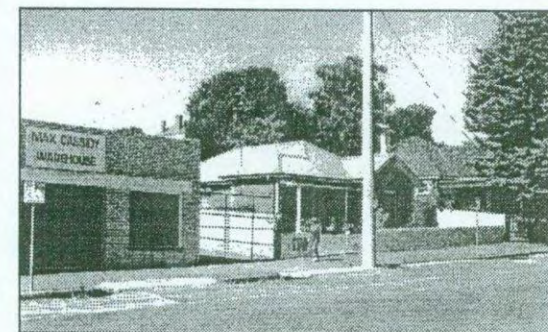


Fig 16: Templeton Street. Potential longer term development site.

4.2 Forest Street /Forest Creek

This precinct covers street frontages along Forest Street from Barker Street to Urquhart Street and includes the site on the west side of Urquhart Street. Forest Creek reserve forms its southern boundary.

This precinct is outside of the Central Core area. Forest Street forms the southern edge of the Central Area.

Existing uses include Western Reserve, tennis Courts, retail, transport and residential.

This site east of Urquhart Street is situated at the entrance to the Central Area. It has an area of 1.2 ha. Existing uses include two dwellings and a TXU depot. Part of the site with frontage to Forest Creek is public land with management vested in the Education Department.

Sites identified for new uses and development are shown on Plan No. 7. See Sites B1, B2 and B3.

Objectives

- Encourage mixed use of the Precinct including recreation, retail and commercial and community facilities.

- Apply a Design and Development Plan Overlay over the entire site to coordinate development and ensure that development generally occurs in accordance with an agreed plan.
- Prepare a Design and Development Plan that identifies building envelopes of new buildings, indicative building form and mass, building lines and setbacks along the street frontage, public space and open space areas, points of vehicle access from Barker Street, and on site car parking areas.
- Retain existing tennis courts and Western Reserve as features of the precinct.
- Retain the landscape character provided by the Oak tree planting adjacent the tennis courts and significant trees in Western Reserve.
- Make Forest Creek a feature of the precinct by upgrading the creek's environmental qualities and appearance.

Strategies

- Use and siting of buildings within the Precinct is to generally conform to the Precinct Concept Plan. See Plan No. 18. Key elements of this Plan are :

* Major new buildings at the eastern and western end of the precinct connected by pedestrian links. Preferred uses to be determined but not to include uses that are dependent on central location.

* A major community facility (recreational or cultural) is recommended for the western end of the precinct at a site generally situated between Western Reserve and Barker Street.



Fig 17: Western Reserve. Site adjacent Barker Street identified for major community facility

* Retention of existing recreation facilities as a feature of the Precinct.

* Retail and commercial redevelopment of the properties with frontage to Forest Street between Wheeler and Urquhart Streets.

- Encourage an alternative use for the Castlemaine Bus Lines building. Preferred uses are retail or business uses that are not dependent on a central location.
- Maintain the mixed residential and commercial use of the row of residential buildings between Wheeler and Urquhart Streets in the shorter term.
- Encourage redevelopment of the properties with frontage to Forest Street between Wheeler and Urquhart Streets. In the shorter term the redevelopment of the Hunt and Lobb site should be encouraged. Replacement buildings are to have a retail or commercial use. Rear as well as main street frontage is encouraged to optimise opportunities for rear vehicle and pedestrian access.
- Provide continuous pedestrian access through the Precinct with links to Forest Creek, tennis courts and Western Reserve.
- Provide for a mixture of retail and public use of the site east of Urquhart Street. Uses should maximise opportunities of

adjacent public open space and Forest Creek Reserve.

- Retain a parcel of land with frontage to the Forest Creek as part of a continuous open space system based on Forest Creek.



Fig 18: Prominent corner site: potential redevelopment site



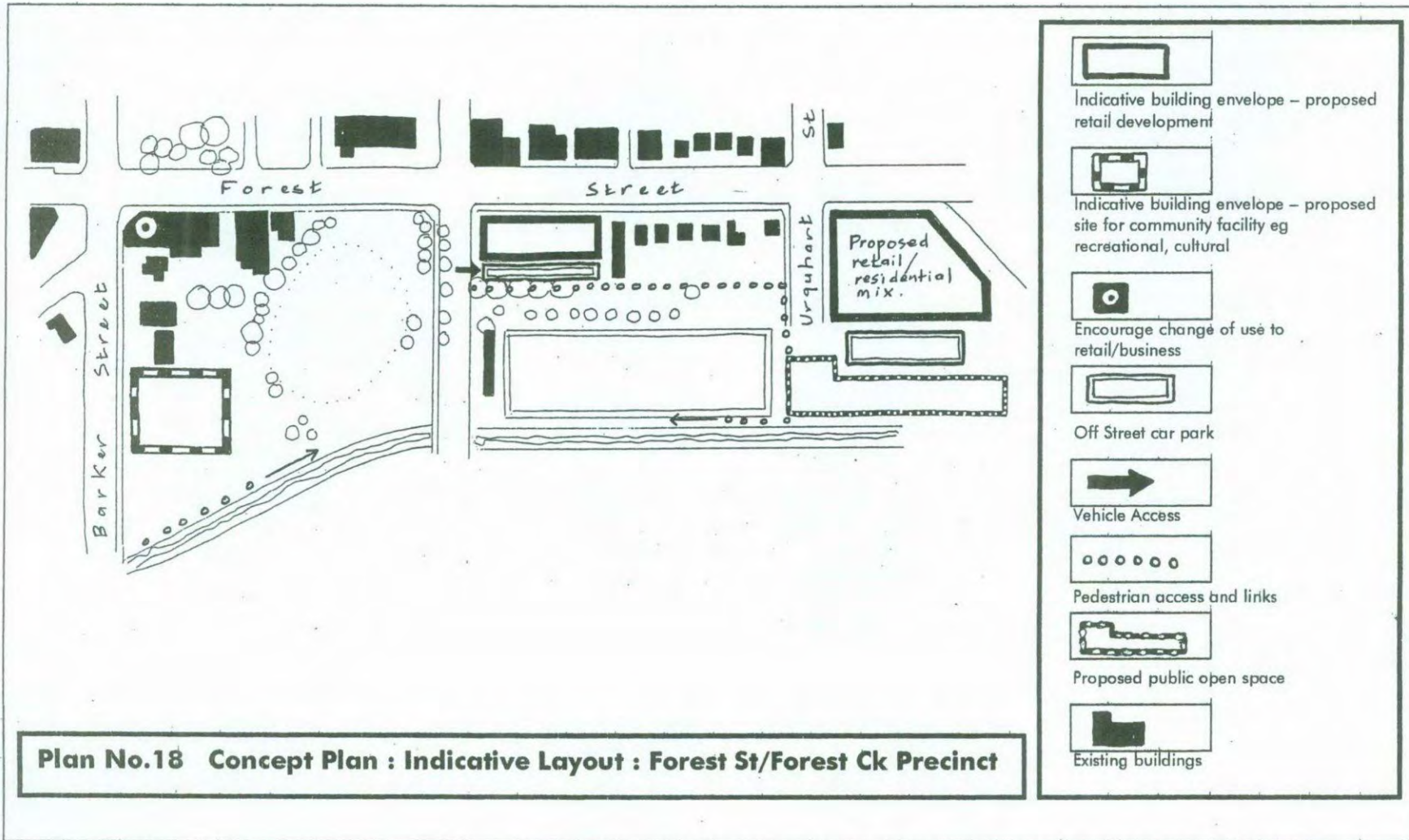
Fig 19: TXU depot: inappropriate use of town centre entry site



Fig 20: Forest Street entry

Concept Plan

See Plan No. 18 , which illustrates proposed location for new building and access through the precinct.



4.3 Barker Street Southern Entry (West Side)

This precinct includes properties located on the west side of Barker Street from Forest Street south to the overhead rail bridge. It is bounded at the north by Bruce Street. Existing uses include caravan park, service station and residential.

The precinct forms part of the important entry to the Central Area.

The ex Steam Flour Mill building is listed on the Historic Buildings Register.

Objective

- Provide for longer term development of the precinct capitalising on opportunities for highway business uses, larger scale development and the full restoration of the ex Steam Flour Mill building.
- Encourage uses that are not dependent on a central location.

Strategies

- Apply a Design and Development Plan covering all land within the Precinct. Design guidelines are to address:
 - * Restoration of and re-use options for the ex Steam Flour Mill building.
 - * Building lines and setbacks to major streets.
 - * Indicative building envelopes and building form and mass.
 - * Points of access from Barker Streets.
 - * Preferred uses for the precinct.
 - * Provision of off street car parking.
- Facilitate the relocation of the caravan park in the longer term.
- Encourage redevelopment of the south west corner of Barker and Forest Streets currently occupied by Scullies Car Sales. Preferred uses for the site are retail or commercial. Uses that depend on a central location should be discouraged. Refer to Site B4 on Plan No. 7.

4.4 Mostyn Street - East End

This precinct has taken on a new life in recent years partly due to the efforts of the Mostyn Street East End Traders Group and the addition of new uses eg Saffs Cafe, Restorers Barn.

The new supermarket has contributed to its busy atmosphere.

The Funeral Directors building at the corner of Lyttleton Place and Mostyn Street has a negative impact on the character of the street. A change of use of the building to one that complements the ambient character of the precinct is preferred.

Two potential redevelopment sites on the north side of the street have been identified (Refer to Site B8 on Plan No. 7 Urban Design Framework Plan.

The row of residential properties on the south side, east of Union Street, could have a mixture of residential and office use.

Objectives

- Enhance the ambient and active character of the Mostyn Street East End Precinct by encouraging active use of premises and ensuring that the design of new buildings and alterations to existing buildings

contribute to the character and form of the precinct.

Strategies

- Encourage the relocation of Thompsons Funeral Directors to facilitate a new use of the building that is more in keeping with the character of the precinct. A more active use of the premises would be more beneficial to the precinct.
- Encourage improvements to be undertaken to the external appearance of the Funeral Directors building to ensure that its appearance is more sympathetic to the streetscape eg. rendering and painting of external walls.
- Recognise the redevelopment potential of two residential properties immediately west of Morrells Gallery in Mostyn Street.
- Future options for new building/s at the above site should be built to the street boundary so as to maintain a uniform commercial streetscape. Preferred uses are active type uses similar to existing uses in Mostyn Street East End.
- Retain residential buildings on the south side of Mostyn Street between Union Street and Urquhart Street and promote this strip as a mix of residential and office uses.



Fig 21: Mostyn Street East end: Potential sites for new retail development.



Fig 22: Obtrusive building in Mostyn Street streetscape: Change of use of building to a use more suited to character and ambience of Mostyn Street recommended

4.5 Barker Street Corridor

Barker Street is an important retail and commercial precinct. It also serves as a main transport corridor passing through the Central Area. The Barker Street precinct has experienced a change in its traditional role since the establishment of the new supermarket in Mostyn Street. In more recent years, it has lost some of its central retail functions and vitality.

Traffic safety for both pedestrians and vehicles is becoming an issue in Barker Street. The high volumes of traffic are producing potential traffic conflict at intersecting streets.

Pedestrian movement across Barker Street is becoming more hazardous. The dedicated pedestrian crossing provides a safe crossing point. Informal crossing using the median strip as a refuge is becoming more difficult.

The location of the existing pedestrian crossing is no longer appropriate. The location served its purpose at the time when key stores such as a supermarket and newsagency were situated adjacent to the crossing on the west side of Barker Street.

The introduction of traffic lights at the intersection with Lyttleton Street is recommended. This would bring the main crossing point closer to Mostyn Street where

central retail activity is now focussed. It would also reinstate the importance of the intersection and the Post Office and Commonwealth Bank corner sites.

Redevelopment of the Commonwealth Bank site at the Lyttleton Street corner and the former Commonwealth Bank site at the north east corner of Barker and Mostyn Streets is recommended.

Sites identified as having redevelopment potential are indicated on Plan No. 7 Urban Design Framework Plan (Sites B6 and B7).

Objectives

- Restore vitality to Barker Street and strengthen its role by improving connectivity within the precinct, redevelopment of key sites, traffic calming measures, and relating its role to the Railway Station and Templeton Street West.
- Identify preferred uses for the west side of the precinct.
- Encourage redevelopment of key corner sites occupied by buildings that are later obtrusive additions to the streetscape.

- Encourage improved rear access for vehicles and pedestrians behind shops with frontage to the west side of Barker Street.
- Plan for a longer term highway bypass of the Castlemaine Central Area to create a safer and more pedestrian friendly Barker Street Precinct.

Strategies

- Encourage a mixture of uses for the west side of Lyttleton Street including retail, specialty stores, commercial and tourism retailing.
- Provide a site for the establishment of a major store type (eg mixed goods) at the northern end of the Central Area in the vicinity of Barker Street.

A site has been identified for a major store (eg. mixed goods) at a mid block location on the north side of Templeton Street between Barker and Hargraves Street. The site (B5) is shown on Plan No. 7. The site is located at the closure of the main Central Core pedestrian link (Frederick lane). At this location, a major store would provide an "anchor" for the northern end of the Central Area thereby adding vitality to Barker Street.

- Encourage redevelopment of the Commonwealth Bank corner. Building form, mass and style to be referenced to original building on this site and to reflect the architectural quality of the Post Office and other important building in Barker and Lyttleton Streets.
- Encourage redevelopment of the north west corner of Barker and Mostyn Streets and including the former SEC building now occupied by CHIRP. Building form, mass and style to be referenced to original building on these sites and to reflect the architectural quality of adjacent buildings.
- Upgrade existing pedestrian links from the Central Core to Barker Street and particularly from activity generators such as IGA supermarket and Market building.
- Development of a Post Office Precinct by creating a public space facilitated by the relocation of the telecommunications building between the Post Office and the former Drill Hall building. Refer to Plan No. 17.
- Establish traffic signalling at the intersection of Barker and Lyttleton Streets with traffic signal phasing giving priority to vehicles crossing and turning into Barker Street and for pedestrians crossing Barker Street.

- Introduce reduced speed limit to improve safety and reduce noise.
- Create a clear impression of entering a major precinct by the use of paving materials in road surface and other hard areas to encourage reduced speed.
- Establishment of a roundabout at the intersection of Barker and Campbell Streets. The roundabout would have a dual function of traffic calming and a Central Area entry statement. The roundabout should include a sculpture by a local artist.
- Encourage VicRoads to undertake a preliminary study to identify options for the eventual highway bypass of the Castlemaine Central Area.



Fig 23: Barker Street Precinct



Fig 24: Lyttleton Street intersection with Barker Street
Traffic signalling recommended at this intersection to replace existing pedestrian crossing

4.6 Barker Street - Railway Station Precinct

This precinct bounded by Barker Street and Kennedy Street includes all street blocks from Forest Street to Templeton Street. It also comprises the west side only of Barker Street.

The features of the precinct include:

- Barker Street retail and commercial strip (west side)
- Railway Station and environs
- Castlemaine Regional Art Gallery
- Heritage Church buildings located on large allotments
- Hilltops and sloping land.

Forest- Mostyn Street Block

The classical Victorian commercial buildings along Barker Street are one of the notable heritage streetscapes in the Central Area. These buildings have heritage protection through listings on the Historic Buildings Register and in the Mount Alexander Planning Scheme.

An existing right of way at the rear of the properties and entered from Forest Street provides an opportunity for rear access and car parking.

Mostyn-Templeton Street Blocks

Major retail frontage along the west side of Barker Street is between Mostyn and Templeton Streets.

The landscape and urban character provided by the sloping land and hilltops is of special interest. Heritage church buildings, the Regional Art Gallery and the Barker Street streetscape dominate the built form character.

Right of way access is available at the rear of Barker Street shops between Lyttleton Street and Templeton Streets.

Informal access is now available at a mid block location running alongside the church buildings and entering Lyttleton Street near to the Regional Art Gallery.

The north-west corner of Barker and Mostyn Streets is occupied by a later obtrusive building. The former SEC building (now

occupied by CHIRP) situated on the adjacent site is also an obtrusive addition to the streetscape. Redevelopment of these sites should be considered as both buildings near their useful and functional life.

Strategies

- Encourage redevelopment of buildings located at the north west corner of Barker and Mostyn Streets and the adjacent building (former SEC building). Refer to Section 4.5)
- Ensure that replacement buildings at the above sites are referenced in their design to the original buildings on these sites and to important adjacent buildings such as the Criterion Hotel. Contemporary architectural expression should also be encouraged.
- Establish a connection between the two existing right of ways to provide a continuous through-block link between Lyttleton and Templeton Streets.
- Provide for improved identification of the through block access to encourage use of this access link to the Regional Art Gallery.

Templeton Street

The strip between Barker and Kennedy Streets that terminates at the railway station is an important entry into Castlemaine and the Central Area. Travellers and tourists gain their initial impression of Castlemaine along Templeton Street. This role for Templeton Street will gain in importance when the Castlemaine -Maldon Tourist train becomes fully operational.

Having arrived in Castlemaine by train, the journey on foot should be made as interesting as possible. This can be achieved by streetscape improvements and by providing a variety of ways to travel on foot to other precincts in the Central Area eg. Central Core, Mostyn Street, Regional Art Gallery.

Objectives

- Encourage a mixture of retail and tourism uses along Templeton Street (between Barker and Kennedy Streets) eg. shops accommodation, cafes and galleries.
- Enhance the streetscape quality particularly the vistas to and from the station.

Strategies

- Encourage the establishment of a continuous through block connection to Lyttleton Street via existing right of ways linking Templeton Street to the Regional Art Gallery and other places of interest west of Barker Street.
- Enhance the visual interest of Templeton Street by establishing avenue planting based on the existing Plane trees and by the introduction of urban and landscape art into the streetscape.



Fig 25: Templeton Street: view to Railway Station

