

May 1998

for Mt. Alexander Shire Council

# Castlemaine Urban Design Strategy





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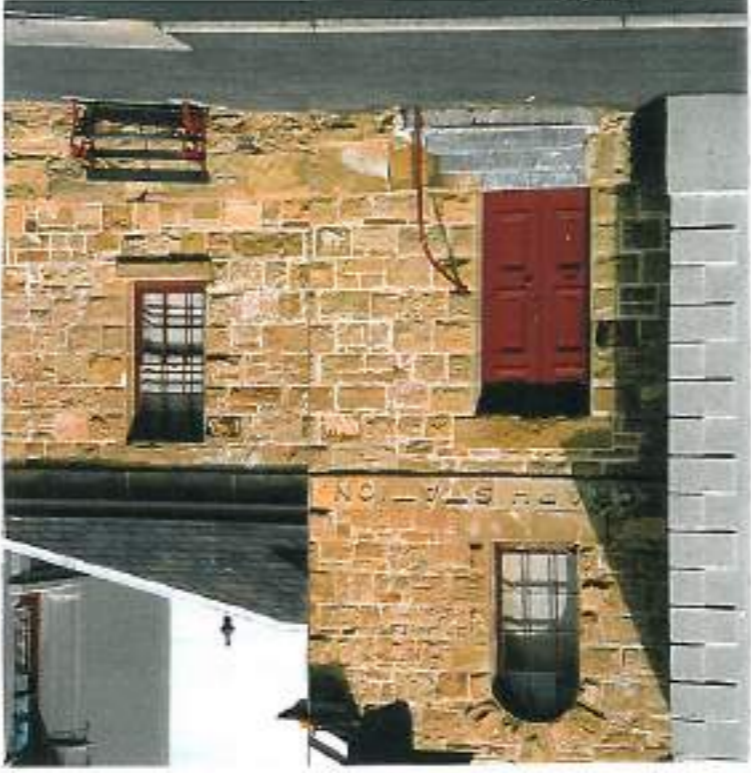
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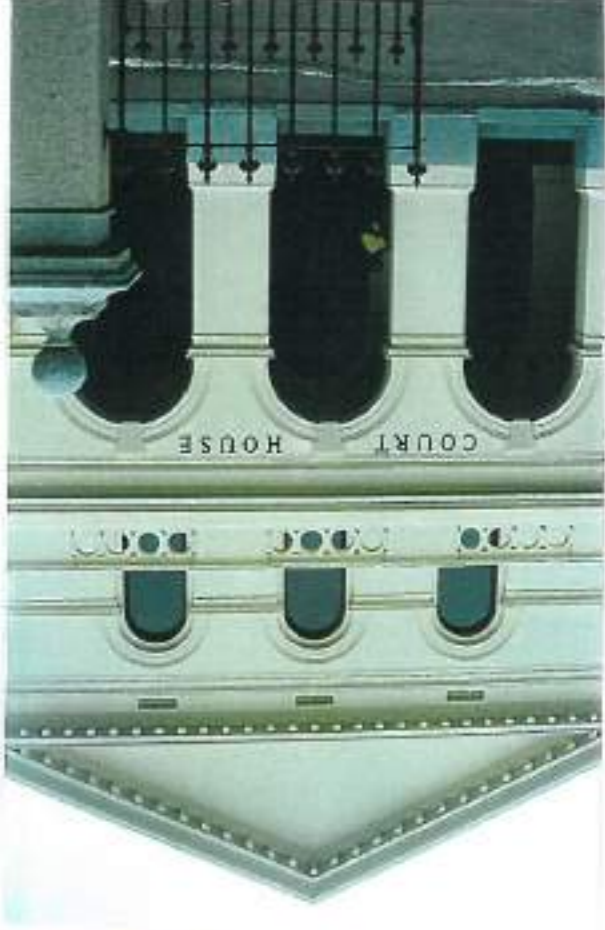
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LANDSCAPE ARCHITECTS URBAN DESIGNERS PLANNERS



Inside cover: the Old Telegraph Building, Barker Street  
and the Court House, Lytton Street.  
cover: Collection of Castlemaine images



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Chapter 1

CONTEXT



top left: Well designed signs on Hargraves Street  
top right: Mostyn Street Canary Island Palms planted as War Memorials  
bottom: Barker Street, February 1998





VISION: SUMMARY OF THE MAIN RECOMMENDATIONS

- this strategic document needs to accommodate review at least on a biennial basis to ensure that changes in community attitudes are not overlooked. This process of review and revision should be formal, inviting community comment
- Castlemaine cannot be thought of as a museum town, it must be a lively place for its residents and those from the region who shop in it and use its facilities; yet it must also sustain and strengthen its appeal to tourists. Tourism is an inseparable part of Castlemaine's economy but it is important that the interests of tourism not dominate the physical character of the town and cause the town to appear as a tourism contrivance. It is the lived in quality of the town and its matter of fact ability of its residents to live amongst its heritage gems, without compromising them, that provides the real attraction for the tourist.
- The heritage protection afforded by the new Planning Scheme be sustained

- The orientation of commercial and public buildings and activity should remain to the street front for this is where the shops are facing, this is where the building facades can be appreciated and this is where the life is. Any move to compromise the street life should be discouraged.
- Traffic should allow for pedestrian considerations to dominate although there is no justification for long term creation of traffic free zones (malls) within the town centre.

- The establishment of defined entry points to the town centre is important given that two state highways continue through the centre. A roundabout is recommended on the north approach to the town centre at the intersection of Barker Street/Campbell Street. A roundabout at this location will significantly improve the definition of the town centre entry from the north and provide a traffic calming function. It will complement the roundabout at the Pyrenees Highway entry point at the corner of Hargraves Street and Forest Street. Further traffic calming measures are recommended for Barker Street in the form of kerb outstands and splitter islands at the intersections with Templeton Street, Lyttleton Street, Mostyn Street and Forest Street (east side only and kerb outstands at the existing pedestrian crossing near the Post Office).
- Tree planting is also recommended within the parking lanes at the junction of parking bays. Fifteen Oriental Plane Trees with traditional picket surrounds are to be planted. The central median should be regressed, irrigated and planted with additional trees to complement existing.

- The southern entry point on the Midland Highway at the south end of Barker Street (south of Forest Street) would also be improved by , reggrading, grassing and Oak Tree planting the area just east of the railway underpass as well as additional planting of Oaks on the west side of the underpass; planting Prunus in the central median, regassing and irrigating the median and planting Claret Ash Trees on the nature strip. Upgrading Western Reserve would also help the image of Barker Street south.

- Further traffic calming measures in the form of kerb outstands and splitter islands should also be undertaken at the intersection of Mostyn Street/Hargraves Street to reduce the crossing distances for pedestrians and vehicles as well as slowing traffic along Hargraves Street.



An important icon is the Patterson Memorial at the corner of Mostyn and Barker Streets.



- Existing outstands and splitter islands at the corner of Hargraves and Lyttleton Street should be redeveloped to simplify them and to introduce trees and other planting of greater relevance to the historic context
- A pedestrian crossing needs to be installed in Hargraves Street to improve safety for elderly residents
- Consideration needs to be given to traffic speeds in Kennedy Street and to measures, which will avoid the street being used as a town centre bypass.
- The Castlemaine Railway Station and Railway shed is another key entrance to the town and as such needs to be given greater prominence, given a more historically relevant landscape treatment as well as expanding its all day carparking and tourist bus parking capacity
- **Carparking:** Castlemaine should maintain and encourage dispersed demand for parking, easy pedestrian access to shops and services and the even spread of demand to reduce peaks. It is considered appropriate that the 30 minute restrictions be progressively changed to 1 hour with the 90 minute restrictions increased to 2 hours. Some ¼ hour parking or ½ hour parking, however, may still be appropriate outside banks, video shops and other facilities, which primarily provide convenience shopping.
- **No large carparks should be contemplated within the town centre except for the upgraded Market Building carpark.**

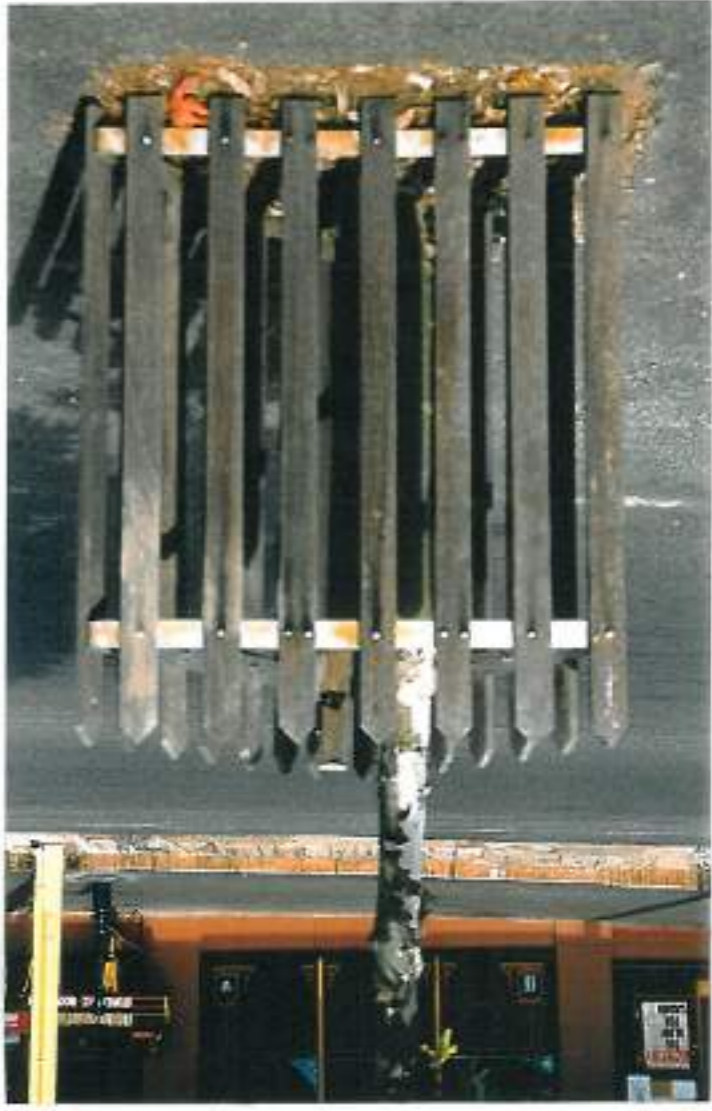
- Any new development in the Civic precinct should be in proportion with the existing group of buildings and the street facades in character with the existing buildings. Spaces between buildings should be roughly in proportion with those, which exist.
- Western Reserve The reserve should be upgraded in a manner which draws it into the town rather than backyards it, as is the case at present. The section of Reserve off Barker Street south lends itself to use as a long-term carpark for cars and buses as well as functioning as a potential open air market and other events site.

**Commercial recommendations include**

- exploiting the gold history of Castlemaine
- exploiting the presence of a major retail food outlet by promoting complementary retailing - maximising efforts to secure a Castlemaine/Maldon steam train service and exploiting the presence of train travellers.
- upgrading and representing some takeaway food outlets to appeal to young people
- exploiting the night time appeal of Castlemaine
- expression and animation of Castlemaine history by retailers exploiting stores and curiosities to encourage walking of the town centre
- consolidating craft product retailers and antique dealers in precincts
- greater emphasis on local produce as part of tourism marketing
- greater specialisation and niche market development especially in the hotel trade, eg, specialist wine bars or night club/coffee lounges, specialised themes such as steam railway, hot rods, gold, Cobb & Co
- proactively encourage the Barker Street North shopping area, as an attractive option to the David's supermarket precinct, centring on the Riteway supermarket.
- greater involvement of young people in all areas of commercial activity, services and tourism
- greater emphasis on appealing to young people via products and their presentation
- selling Castlemaine's commercial strengths to entice new businesses not currently in the town
- improved tourism oriented signage at Calder Highway
- improved long term and tourist parking options
- improved appearance and facilities in Western Reserve to encourage its use for events and



Top: Western Reserve could be an appealing complement to the town centre.  
 Centre: Barker Street, February 1998 with a 'suburban' appearance.  
 Bottom: Picket surrounds – an essential feature of street tree planting in Castlemaine.

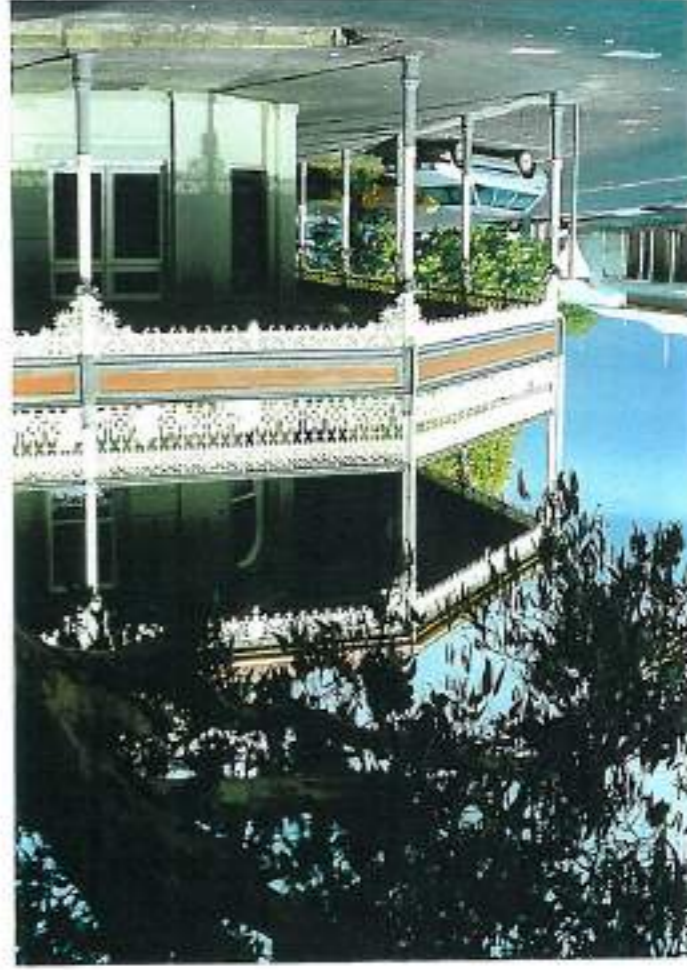




- Introducing traffic calming measures and making other improvements to Barker Street to promote weekend markets
- enhancing the image of the Civic precinct by lighting and appropriate street furniture
- better planning of shop opening hours to reflect experience and the needs of niche markets
- night time opening of the Art Gallery/Museum during tourist peaks to complement other night time interests.

- **Street Furniture.** We recommend that street furniture be historically authentic except where details were not in use eg to accommodate necessary changes to traffic and pedestrian movement and to accommodate works of art however, we recommend avoiding details which clash with scale and colour of heritage buildings or other known streetscape details. In general we recommend replacement of any inappropriate street furniture and for the streetscape advisory committee to recommend locations for additional furniture in consultation with traders where necessary.

**Note :** For details of the above recommendations and for other minor recommendations refer to the body of the report.



**top :** Shire of Mt. Alexander Municipal Offices, Lytleton Street, March 1998.  
**bottom :** Midland Private Hotel, Kennedy Street, March 1998.



CHAPTER 1: CONTEXT

1.01 Purpose of the Study

The purpose of the study is to develop a long-term *vision* for the Castlemaine Town Centre. The *vision* will have many parts and it will be a composite view of the community, its many advisers, elected Councillors, Council staff and government agencies, it is not just the view of the consultants. Council aims to revitalise the commercial area and as part of that process it needs to identify a streetscape works program based on priority needs. It is the strategy, which identifies the needs and the priorities. The strategy will also guide Council and the trading community when making decisions regarding almost any works or activities that could affect the vitality and appearance of any part of the Town Centre in the foreseeable future. The strategy is also a fundamental prerequisite for Pride of Place capital works funding and would also assist with funding applications to other fund sources. Many issues are considered including the town's historic character and the evolution of that character, current needs to meet the town's role as a service centre, needs of the visitor, commercial imperatives, management and maintenance of the town and a concept of what the community expects the town to be like in 10 to 15 years time.

1.02 The Study Area

The study area is termed the "Central Business Area" (CBA). It is bounded by Forest Street, Kennedy Street, Campbell Street and Urquhart Street. Some issues overflow beyond the study area, such as the Railway Station, Western Reserve and the Midland Highway and Pyrenees Highway entrances to the CBA. Accordingly these areas and places have been added. Some parts of the study area are residential or institutional, not commercial, however, they can be affected by parking and traffic.

1.03 The Castlemaine Context

Castlemaine is a very appealing historic town, but it faces some dilemmas. How can its visual appeal be maintained when the pressures of the 21<sup>st</sup> century are brought to bear? How can Castlemaine provide for the contemporary needs of its residents and continue to attract tourists? Why do tourists come, what do they want to experience and how can those experiences be provided without changing the character and integrity of the town?

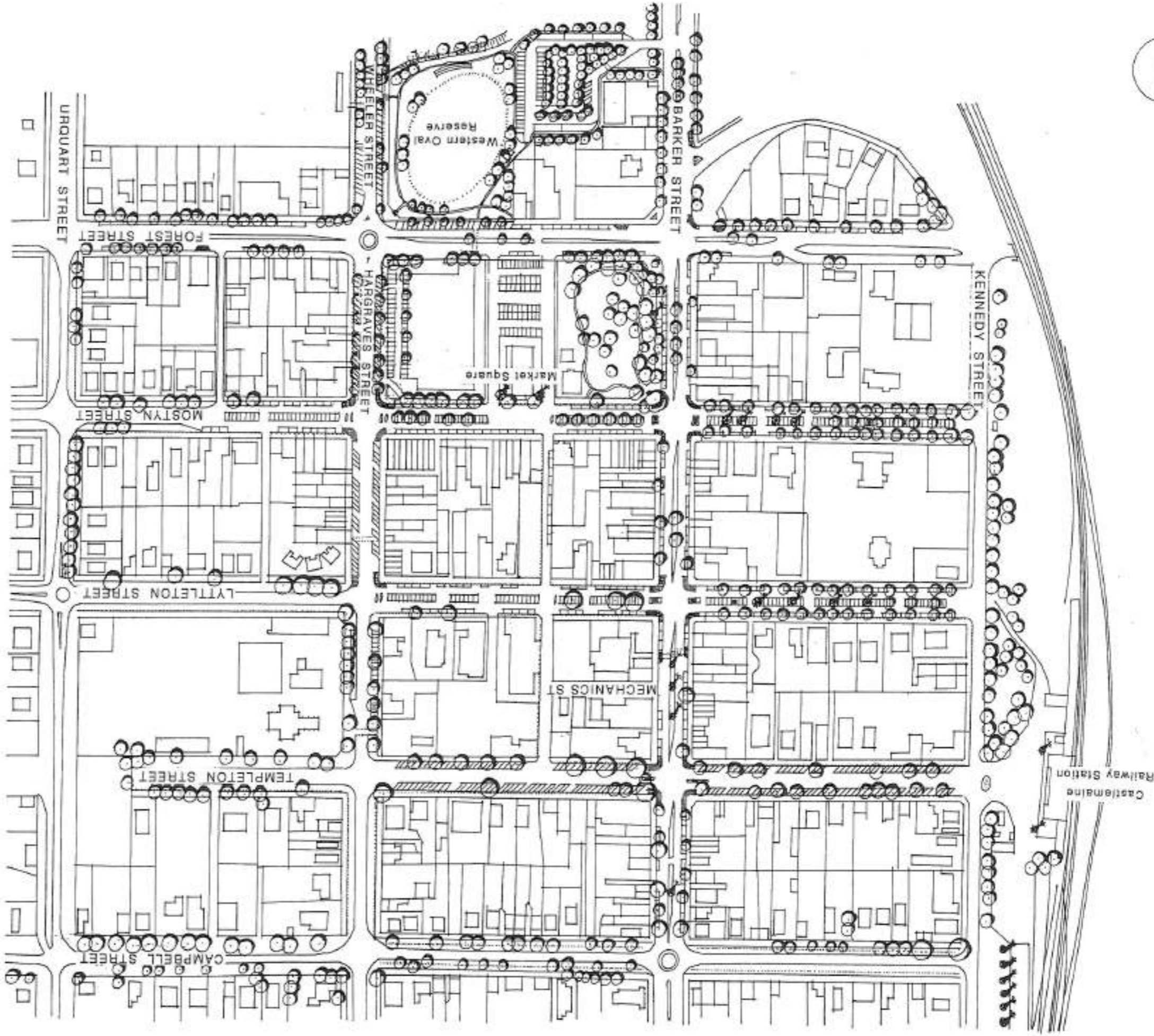
Castlemaine enjoys a gold created heritage and a very fine collection of 19<sup>th</sup> Century buildings. Its commercial and civic streetscapes are dominated by 19<sup>th</sup> Century built form and visually it remains a convincingly historic town. Castlemaine enjoyed a rich period in the 19<sup>th</sup> Century when money was invested in a physical form that is still largely intact. It has also indirectly benefited from a long period of low investment, from the time the gold ran out and therefore, there has been little pressure to redevelop or otherwise change its building stock. Bricks and mortar richly decorated and of a nature beyond the budgets applied to such buildings in the 1990's provide Castlemaine with an extraordinary environment much loved by its community and a highly marketable asset. Add to the building stock, some fine trees and monuments and it would seem there was a basis for thrive rather than merely survive. So what is wrong?



top left: Cast iron bollards should be restricted in use to the civic precinct, rather than in general commercial areas. top right: Many of Castlemaine's commercial signs are 'suburban' in character. There are better examples to be found in Hargraves Street.







STUDY AREA



The presence of the Midland Highway and the traffic it carries has both positive and negative effects on the town, with the positive probably outweighing the negative given the comparatively low total traffic volume the highway carries. The physical demands of a highway nonetheless compromise a commercial streetscape with its parking, safe and slow traffic movements and pedestrian needs.

Another problem affecting traders and building owners is that the building stock is costly to maintain and costly to renovate to accommodate contemporary internal needs and outward presentation. This fact either adds a financial load onto the trading community or there is progressive decline of the building.

The sense of "old" is also not enough in a rapidly changing, demanding market. There are many commercial strip shopping centres in Melbourne with a similar sense of old but these places also need to cater for the new age needs, some of them quite successfully eg the commercial areas of South Melbourne, Albert Park, Hawthorn, St Kilda, Fitzroy, Carlton and Moonee Ponds. Perceived Community needs and accessibility to alternative and more complex commercial markets such as Bendigo and Melbourne, via good roads and trains, makes life very difficult for some local traders trying to provide the Castlemaine community with retail choice and having a much smaller catchment than the examples above. Unfortunately, the community does not sense the importance of supporting and strengthening their local shops as an essential aspect of providing employment and conserving the towns vitality.

What about the tourists? There is evidence that tourists interests and trends are changing rapidly. The fascination with history is no longer enough for the younger people who have been raised with television, Sovereign Hill, Internet and virtual reality. History needs to be packaged differently in the late 20<sup>th</sup> Century whether we like it or not.

Young people may wonder what the appeal of Castlemaine is, a collection of old buildings with little animation, little to explain its history and not much to do. The older visitors are still there and they still visit but in fewer numbers and the likelihood of repeat visits is becoming less. There is a great deal of competition for the tourist dollar, throughout the region and many clever ways of helping them to spend time places other than Castlemaine. Nonetheless, Castlemaine has many strengths that other townships would envy, especially its strong sense of history, its gaol, railway station, artistic resources, its accessibility from Melbourne, its botanic garden, Buda and its gold. It also has some good industries and a central location allowing easy access to many other places. These strengths should not only appeal to the tourist but also to those who may be looking for places to retire or to raise families.

This strategy needs to address all of these issues and a multitude of sub issues. Finding a solution for any one aspect is however, not enough, it is necessary to understand and deal with many parts just like a well-maintained motor car.

*Castlemaine cannot be thought of as a museum town, it must be a lively place for its residents and those from the region who shop in it and use its facilities; yet it must also sustain and strengthen its appeal to tourists, this must underlie the vision. Most European towns that have tourist appeal also function well for their residents and it is often the sense of community vitality, which strengthens a town's attraction for its visitors. This is particularly so with Victorian coastal townships Sorrento, Queenscliffe and Port Fairy.*

The strategy has considered and made recommendations concerning:

- the overall visual character and the character of each street and sections of street within the study area
- planning issues and continued heritage protection for buildings and landscape
- traffic circulation and parking

Canary Island Palms were widely planted after World War I as a memorial. They have a strong presence in both Mostyn and Lyttelton Streets and around the railway station.





- appearance of the Town Centre entrances
- improvements to the commercial shopping streets ( especially in response to the new supermarket)
- pedestrian and cycle movement
- streetscape elements such as trees, seats, litter bins, bollards and signs
- the role of artworks
- lighting
- tourism which includes aspects such as - the overall visual image of Castlemaine, the appearance of shops, tourist facilities and interpretation of the town's features, tourist parking (especially for buses and cars with caravans and trailers)

There have been many good reports, plans and ideas already put to Council and to its predecessors. We are considering these along with the community feedback. We are also familiar with the details of the Market Square precinct and the L19 agreement.

#### 1.04 The Brief

The brief makes it clear that Mount Alexander Shire Council "has retained strong links with its historical past and intends to structure its medium to long term development around tourism and recreational opportunities.

The scope of the strategy includes

- Urban Streetscape Design Analysis (Castlemaine central Business Precinct) - Issues and Opportunities
- Identify issues and opportunities relating to range of factors including pedestrian movement patterns, traffic circulation and conflict, car parking, links to adjoining areas (eg. railway station, administrative/cultural precinct), Central Business Precinct entries, streetscape elements (street furniture, signage, lighting, landscaping, pavement surfaces, public art), tourism promotion, heritage protection.
- Undertake consultation with traders
- Identify strengths and weaknesses
- Identify strategies and opportunities to improve the overall retail performance of the precinct
- Identify areas within the precinct that need to be targeted for urban design improvement
- Assess implications for adjacent areas and precincts
- Identify opportunities arising from the new supermarket and Market Square developments
- Identify opportunities to achieve retail integration with Market Square Precinct.

A Traffic and parking management plan is a major part of the Urban and Streetscape Design Framework. Traffic calming and pedestrian safety in all streets and protecting the highway function of Forrest Street and Barker Street are all-important issues.

The Traffic and Marketing Management Study will: -

- Draw together appropriate elements of previous relevant studies
- Recommend traffic management measures to reduce travel speeds.
- Review and make recommendations in relation to carparking layout and operation, with a view to maximising car parking provision within the precinct
- Improve pedestrian safety and accessibility
- Recommend a traffic gateway treatment at the main entries to the precinct
- Recommend appropriate treatment for Barker Street intersections

Implementation of design improvements will be set out clearly and will include:-

- staging, timing and priorities for capital works



Victory Park through an inviting entry.



- cost estimates for all stages
- detail costing for stage one
- funding options to progressively finance total capital works

### 1.05 Consulting the People

During the preparation of this strategy, there were several ways in which the project team consulted the community:

The Council formed a consultative committee comprising elected Councillors and trader representatives and there were several meetings held with this committee to identify the issues and to discuss some ideas that had been previously proposed by Council and by community groups.

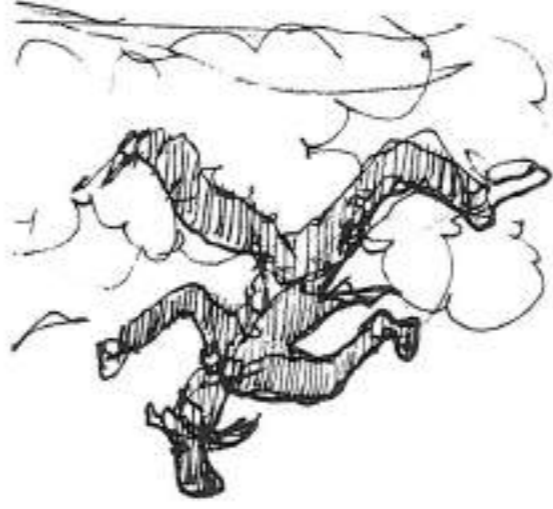
The second contact was by discussion with individual traders and other Councillors representing the wider community interests.

The third contact was at an exhibition of the first draft of the strategy. This exhibition attracted approximately 130 people and many constructive comments were received and acted on in our recommendations.

Many people also telephoned our office and made additional comments, these were given similar consideration and if not already covered, the suggestions were included in the strategy.

The fourth contact was an invitation to comment on the Final Draft. Comments would again be considered and the report amended as per advice from the Steering Committee.

Given the comparatively tight programme, it could be argued that there was never enough time to properly consider such an important document. However, we see this document as one which needs to accommodate review and in fact there needs to be at least a annual review to ensure that changes in community attitudes are not overlooked. This process of review and revision is also part of the vision







## Chapter 2

### PLANNING



CHAPTER 2: PLANNING

2.01 Planning Controls and Issues

At present the study area is covered by a number of different zones. The core area of the CAD is located within a Central Business or Service Business zone. In addition, there are land holdings in the Special Use Zone to the west of the precinct and a number of properties (comprising the main public buildings such as the Library, Town Hall, Court House, Post Office) that are located in the Public Purposes Zone. A small pocket of land is included within the Township Living Zone. The R2 Township zone covers the northern section of the CAD between Campbell Street and Templeton Street.

The controls for Areas of Special Significance also apply to a number of properties in the central city area, essentially these cover significant Heritage buildings.

The objectives and requirements for the Areas of Special Significance apply in addition to the requirements under the current zoning's and reservations. The key matters for consideration include the desire for the preservation of significant sites and landscape features which have been recognised as important by Council, and those items which form an essential component of the heritage and character of the area. Any development should complement the streetscape and any proposed guidelines should not conflict with these objectives.

New format Mt Alexander Planning Scheme provisions

For most of the CAD Area in Castlemaine the proposed zoning is the Business 1 Zone which accommodates the existing uses and represents a direct translation from the existing zone. The proposed planning scheme also includes a *Design and Development Overlay (DDO1)* for this area.

Importantly the DDO1 specifies certain matters that must be considered in the assessment of planning applications. These will not be repeated here in full. Particular emphasis is on the preservation of the existing character of Castlemaine's streetscapes and public buildings.

In addition, under the heading "Decision guidelines" at Clause 3.0 to Schedule 1 to the DDO1 overlay, are a number of guidelines which need to be considered when dealing with a planning application.

Status of the new Mt Alexander Planning Scheme

The new Planning Scheme has been exhibited and an Independent Panel appointed by the Minister heard submissions in March 1998. Council has advised that there are no significant changes that would affect the study for the CAD.

Once adopted the Design and Development Overlay (DDO1) and Environmental Significance Overlay NO 8 - Highway Protection Environs will come into effect.

Planning Processes

At present the Planners follow the planning application process in accordance with the provisions of the Planning and Environment Act. It seems that they are mindful of the need to act promptly for the attraction of new business to the CAD.

In this regard an application generally takes between 1 and 2 weeks to be processed. Given the nature of the application likely within the CAD, this timing is satisfactory. Generally no formal

**top:** This building is an example of the stripping of the 19<sup>th</sup> Century character and verandahs that are essential features of Castlemaine. Buildings such as this should be restored or otherwise altered to suit the dominant street character.  
**bottom:** The Pyrenees Highway and the Midland Highway should not impose a 'highway' character on the town centre. The Urban Design Strategy aims to maintain the capacity and function of the highway but it's visual character would be appropriate to the shopping and pedestrian needs and interests.





advertising is necessary as the main applications include change of use applications, minor alterations and additions to existing buildings and signage for new businesses.

For new applications an applicant is told that a planning permit is required and that it will need to be referred to a Heritage Adviser. Sending a facsimile to the Heritage Adviser for assessment and analysis facilitates this process. The outcome and decision lies in the hands of the Council officer and the Heritage Adviser.

At face value this process appears to be working satisfactorily however there appears to be some areas where improvement could be made to assist in the customer service for the traders and the community generally.

The two recommendations are as follows:

- Consideration be given to the preparation of a handout for customers (applicants) indicating the necessary materials for lodging an application. The inclusion of some basic information about the process would also be useful. The preparation of this type of brochure would attempt to provide a greater understanding of the process as well as the necessary requirements to gain approval. It may also assist by minimising delays in the approval process.
- Providing a service at the 'heart' ie, in Castlemaine, would substantially improve the service to traders. At present simple queries would take an average of 1 hour out of the daily work time due to the need to travel to Newstead and back. The provision of planning and building staff at Castlemaine would assist with the direct provision of advice and assistance.

## 2.02 Strategic Urban Design Issues

Most of the detailed design issues are covered in chapter 4 however, there are some 'big picture' issues which need to be addressed.

- The most significant issue is that at present there is not a sufficiently convincing sense of an important historic town at the key arrival points. In each town centre entry place, Forest Street from Urquhart Street westward, Barker Street, from the southern entry and from the Campbell Street intersection heading southward, the character is compromised by the highway infrastructure. Concrete kerbs replace stone, the alignment of kerbs relates to traffic needs, the lane delineation, the shape and appearance of central medians is of a highway vernacular, the lighting, similarly follows the highway vernacular. On both streets, the highway character is maintained at the expense of a preferred historic town centre character. There is a strong need to redress this problem in proportion with the likely available funding and the needs of other parts of the town.
- The Pyrenee Highway (Forest Street) visually separates Western Reserve from the Market Building/Victory Park area; this perception needs to be reversed and more frequent use of the Reserve should be encouraged. Some of the period street furniture is not appropriate to general use because it has not evolved with the other components of Castlemaine's physical fabric. The choice of street furniture, lighting and signs needs to relate to the historic character of Castlemaine. The style of such furnishings should be peculiar to the town (as illustrated in historic photos), to the materials which were commonly used and to the manner in which they were crafted. Importing furnishings that belong in Bendigo, Melbourne or in Ballarat irrespective of their appeal or otherwise, does not help to reinforce the historic individuality of Castlemaine.
- The existing stone kerbs and gutters are a fundamental aspect of the town's sense of history. Although they may not necessarily conform with preferred intersection treatments or with



These highway views need to read as town centre entries. This would be achieved by planting Oriental Plane trees to create an avenue and by upgrading the quality of grassing and planting in the centre median.





contemporary drainage dictates, they need to be conserved, well maintained (including removal of bitumen) and treated as a fixture. Other traffic, parking and access needs should be determined without compromise to the stone kerbs and drains.

- Lighting is not considered adequate in some parts of the town however, it is not desirable to introduce major changes to existing street lighting. New lighting should be subtle and relevant to localised needs as covered in the Design details.
- There is a need for more effective use of art works in the town, particularly to give expression to the gold history, the undeniable reason for the town's existence. The proposed sculpture for the new roundabout at the intersection of Forest Street and Hargraves Street is a good example of how Castlemaine's history can be given life and greater meaning for its visitors. Similar gold related sculpture would be appropriate for the Campbell Street/Barker Street entrance to the town centre.
- The Civic precinct warrants some guidance when future development is considered for the area north of the Town Hall/Mechanics Institute/Court House.

Any new development should be in proportion with the existing group of buildings. Spaces between buildings should be roughly in proportion with those which exist around the above buildings, the Drill Hall, Library etc.

- Consideration should be given to linking pedestrian movement around the civic buildings and to the creation of simple comforts such as occasional seating, lighting and shelter.
- The railway Station is another key entrance to the town and as such needs to be given greater prominence by removing the screening vegetation and introducing simple planting relevant to the period in which the station was built - see Appendix.
- The carpark needs to be redesigned to better accommodate buses and more long term parking, but the redevelopment should not be done in a way which compromises the historic character of the station precinct or clutter the space in front of the building with parked cars or buses. The station buildings should also be floodlit to enhance their night time appeal.

- There has been concern expressed that the centre median carparking in Mostyn Street is in conflict with the purity of the axial vista between the Burke and Wills monument and the War memorial. While we understand this point of view, we do not believe the axis is easily read or that either monument is substantial enough to support such an axial concept. We believe the town is better served by maintaining this parking and by planting Oriental Plane Trees to shade the parked cars.

### 2.03 Pedestrian issues

Castlemaine has a traditional, symmetrical, planned street pattern with a concentration of retailing in Barker Street and Mostyn Street. It has civic buildings in Lyttleton Street, Mostyn Street (Market Building) and Barker Street all of which, with Victory Park and some important monuments provide interest for the walker. The town is easy to walk but should be made more interesting and more inviting both night and day (see Tourism Strategies). Such initiative would become a major selling feature of the town.

The emphasis, nonetheless, should remain on the street front for this is where the shops are facing, this is where the building facades can be appreciated and this is where the life is. Any move to compromise the street life should be discouraged.

Any proposals for significant public squares or plazas, which are not on the street front, should be discounted in order to retain the street front spaces for such purpose. Examples are Market Square, sidewalk cafes, hotels, the Municipal Building, the Courthouse and the Old Telegraph building. Traffic and Carparking should allow for pedestrian considerations to dominate although there is no justification for creating traffic free zones within the town centre. Parking times should be short term (90 mins max) rather than all day, in the interests of shoppers. All day parking should be kept to the perimeter of the town centre eg, at the railway station and at Western Reserve.

No large carparks should be contemplated within the town centre except for the upgraded market Building carpark. Concentrated parking elsewhere can aggravate traffic problems especially at



The railway station is integral with the town centre and needs to be given greater visual importance by improvements to planting (see appendix).



The road network within the Castlemaine CBD consists generally of a grid of streets with classifications from access through to state highway. The grid network allows excellent accessibility through the CBD, helping to efficiently distribute traffic flows and provides circulation opportunities. With the exception of the two state highways, all streets have a high focus on on-street parking activities, which provide impediments to through traffic and high speeds.

By contrast, parking activities along the two state highways (Barker Street and Forest Street) are restricted at present to parallel parking. This ensures that the right lane of each carriageway is not impacted by vehicles manoeuvring into and out of parking spaces, preserving their through function role.

In terms of traffic flows, Barker Street carries in the order of 9000-10,000 vehicles per day; while Forest Street carries in the order of 6000-7000 vehicle per day. Commensurate with being an entry to the CBD, Hargraves Street carries around 5000 vehicles per day at its southern end. This compares with Wheeler Street, which has a volume of around 3000 vehicles per day south of Forest Street.

Traffic surveys undertaken by VicRoads also indicate that Barker Street is carrying around 9% heavy vehicles, which is typical for arterial roads. The 85th percentile speed is in the order of 50km/hr. A review of state accident records between 1991 and 1995 indicate that almost all casualty road accidents within the CBD occur on or around Barker Street. This is illustrated in the Appendix

**Traffic Management**

**2.05 Traffic and Carparking**

Consideration needs to be given to cycle access requirements when detailing traffic control measures and when determining where cycle stands should be provided. Sign posting to advise preferred cycle routes, avoiding hazardous intersections could also be considered.

Castlemaine is generally suitable for cycling and access should be made easy almost wherever cyclists wish to go.

Information for cyclists should be available at the railway station and it may be desirable for the station to be seen as the starting point for cycle tours (call it the "cycle hub") whether cyclists are arriving by train or by car. Sign posting could direct visitors to the "cycle hub" from the Midland and Pyrenees Highways.

Castlemaine is already a favoured destination for cyclists but it could become more so if the facilities were improved. Cycle tour information should be provided including regional destinations, travel times, best routes and road conditions, cycle repair and parts, off road tours for mountain bikers, points of interest and facilities at each destination such as camping and picnic sites. Such information should be made available at the information centre and the railway station.

**2.03 Cyclist Issues**

Intersections at peak movement times and have the potential to aggravate pedestrian movement and demand to reduce peaks. For example, any traditional peaks such as rate paying deadlines should be set for low traditionally low demand days and times.

The clutter of cars and trucks creates uninviting territory for pedestrians. Tree planting within the parking lane will lessen this problem.





Drawing No Traffic 1, depicts all accidents in the study area. Importantly, none of the accidents involved heavy vehicles.

The highest concentration of accidents occurred at the intersection of Moyston Street and Barker Street, and involved several pedestrians. With Moyston Street being one of the prime retail strips, this intersection attracts a high number of turning vehicles as well as pedestrians. Accordingly, there is an urgent need to improve safety at this intersection.

**Opportunities**

In general, the primary road network in Castlemaine is characterised by very wide (30 metres) reservations. These large reservations provide ample opportunities for on-street parking. However, they also result in very wide intersections.

The provision of large intersections allows traffic management treatments such as the existing right turn lanes and proposed roundabout at Forest Street/Hargraves Street to be accommodated. Care, however, needs to be taken to also ensure that the safety of pedestrians is not compromised.

In this instance, opportunities exist to reduce the overall size of most intersections to reduce the crossing distances for pedestrians and vehicles. This reduces their exposure to conflict and also improves their visibility and sight distance beyond parked cars

This can be achieved by the use of kerb outstands around each corner of the principal intersection as shown in the Appendix. Kerb outstands should be designed to bring the pedestrian or vehicle in line with parked cars, while keeping parked cars far enough from the crossing point to ensure visibility to motorists.

Extending median islands further into the intersection allows pedestrians to stage their crossing. Kerb outstands and median extensions will also impact vehicle speeds by reducing the available turning radius. However, care must be taken to ensure that the town centre remains accessible to delivery trucks and service vehicles, which have larger turning circles than cars.

Kerb outstands and splitter islands are also recommended at the intersection of Moyston Street/Hargraves Street to reduce the crossing distances for pedestrians and vehicles as well as slowing traffic along Hargraves Street.

An opportunity also exists to reduce the impact of truck deliveries to Sandhurst in Barker Street near Templeton Street by providing a break in the central median along Barker Street opposite the driveway. Currently trucks are forced to mount the median to enter the property from the north. The removal of the median at this location will reduce delays to other motorists and reflect existing operations.

Kennedy Street will need consideration given its potential use as a Midland Highway by pass by locals. Traffic speed may warrant installation of central median islands and outstands at intersections to allow safer pedestrian crossing and to reduce traffic speed.

**Gateways**

The establishment of defined entry points to the town centre is important given that two state highways continue through the centre. Motorists travelling on these roads are generally in two categories:

- Through traffic; and
- Terminating traffic.



**top:** Kennedy Street needs consideration to reduce the traffic speed and reduce its use as a Barker Street bypass.  
**centre:** More tree shaded parking needs to be provided in Mostyn Street west end.  
**bottom:** The concrete section of the centre median needs to be reconstructed to improve truck access. The grass areas need to be upgraded to improve the visual quality of the median.





Both groups have probably been travelling at high speeds on approach to the centre. However, while they will no doubt have noticed the change in land use and density, the through traffic, in particular, may not fully appreciate that the highway takes them through a high activity zone. As a consequence, it is important that gateways to the town centre be established on both of these roads which not only provide a visual identification of the activity centre but also interrupts their travel behaviour by requiring some active response.

On the south approach to the town centre along Barker Street, this is achieved actively by the existing signalised intersection. However, some visual treatments will also assist. The traffic signals, while controlling vehicular and pedestrian movements at the intersection also placates north-bound traffic through the CBD, improving opportunities for traffic to cross Barker Street.

The proposed roundabout at the intersection of Forest Street/Hargraves Street/Wheeler Street, which is about to be constructed, will provide both the necessary visual and active gateway treatment on the east approach. The roundabout allows the streetscape to extend into the centre of the intersection, while slowing motorists on all approaches.

The roundabout also provides median islands for pedestrians to stage their crossing in a low speed environment.

A similar treatment is also recommended on the north approach to the town centre at the intersection of Barker Street/Campbell Street. A roundabout at this location will significantly improve the definition of the CBD entry from the north. However, given that Campbell Street is primarily a residential street, it will be important to ensure that the design discourages heavy vehicles from entering the street.

In this respect, the rural cross-section of Campbell Street and the general impedance to motorists due to angle parking along Hargraves Street are likely to naturally discourage through traffic from using this route. However it is noted that some terminating traffic may find this alternate route attractive. Notwithstanding this, the volume of traffic using Campbell Street is not expected to cause a significant impact on its amenity given:

- Barker Street leads directly into the main shopping precinct; and
- Campbell Street has a generous cross-section, which can easily cater for some additional traffic.

Similarly, through traffic is not expected to use Urquhart Street due to its vertical geometry.

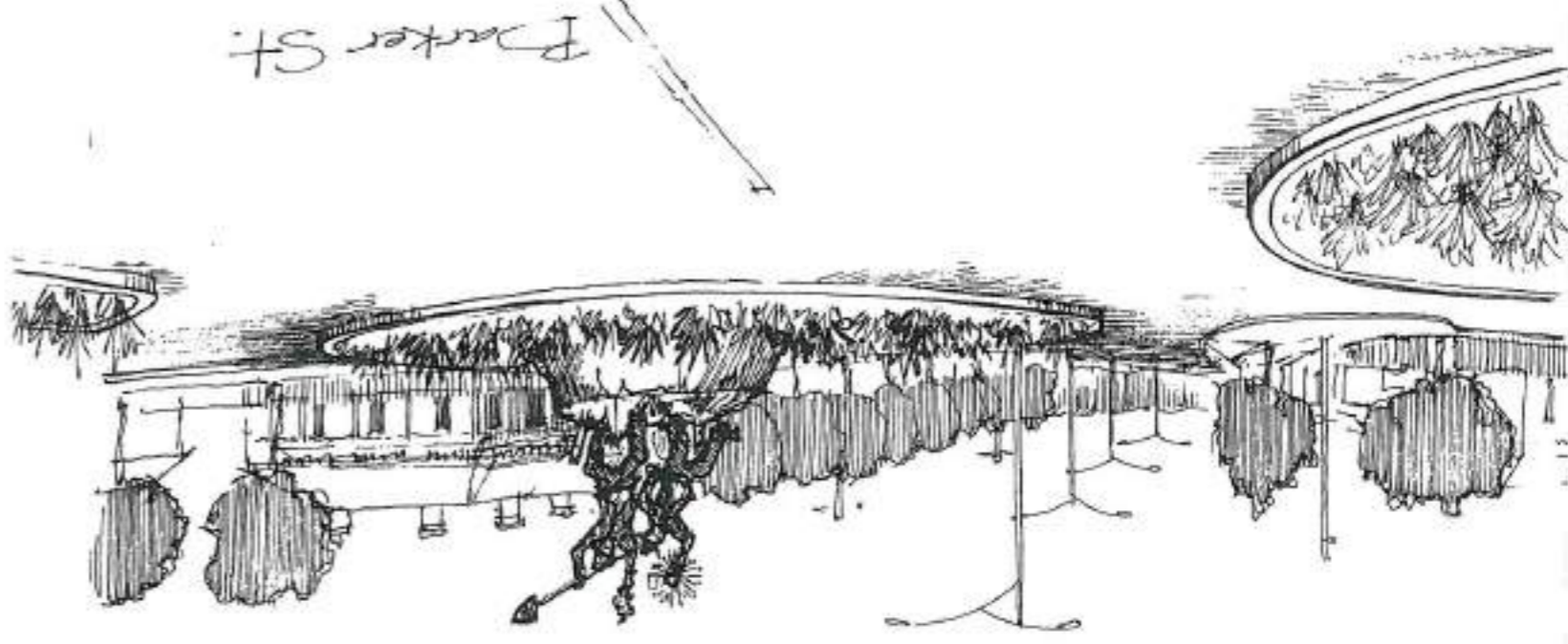
**Recommendations:**

To improve traffic control through the centre and improve safety for pedestrians the following traffic management treatments are proposed:

- Install kerb outstands at the following intersections:  
 - Barker Street/Moyston Street *High Priority*  
 - Barker Street/Lyttleton Street *High Priority*  
 - Barker Street/Templeton Street *Low Priority*
- Install kerb outstands and splitter islands at the intersection of Hargraves Street/Moyston Street *Medium Priority*
- Install kerb outstands at the signalised pedestrian crossing on Barker Street, near Lyttleton Street, as part of intersection treatment at Lyttleton Street *High Priority*
- Install roundabout at intersection of Barker Street/Campbell Street *Medium Priority*  
 (Roundabout design to discourage use of Campbell Street)
- Provide a break in the Barker Street median opposite Sandhurst *Medium Priority*



**top:** Site of the Forest Street/Hargraves Street roundabout to be constructed in 1998.  
**bottom:** Artist impression of possible roundabout at the intersection of Barker Street and Campbell Street.





**Parking Management**

- Existing Conditions

Car parking within the CBD consists primarily of on-street parking with some off-street parking, both private and public, also available.  
 Both the existing supply and demand is thoroughly documented in a report for Mount Alexander Shire Council, prepared by Andrew O'Brien and Associates Pty Ltd in January 1997. This report is titled 'Castlemaine Town Centre Parking Study – Parking Report'.

- On-Street Supply And Management

There are three typical configurations of on-street car parking:

Parking Configuration	Location
Parallel parking (kerbside)	Forest Street, Barker Street, Frederick Street
Parallel parking with centre-of-road parking	Moyston Street, Lytton Street
Angle parking (kerbside)	Hargraves Street, Templeton Street

In general, the kerbside parking is restricted to 30-minute parking while the centre-of-road parking is restricted to 90-minute parking.

- Off-Street Car Parks – Supply And Management

The main off-street car park in the CBD is the Market Building Car Park. This is shortly to be joined by the adjacent supermarket car park under development, providing in the order of 200 (combined) number of spaces. The Market car park primarily provides all day parking.  
 The Stoneman's supermarket car park on Frederick Street south of Lytton Street contains some 25 spaces with 90-minute restrictions.

- Parking Demand

The Andrew O'Brien Study revealed high parking demands in the following areas:

- Moyston Street, between Barker Street and Hargraves Street – centre-of-road;
- Stoneman's car park;
- Lytton Street between Frederick Street and Barker Street;
- Barker Street, west side between Moyston Street and Lytton Street;
- Templeton Street, west of Barker Street;
- Rear of Council offices.

While some over-staying occurred in the restricted areas, highlighting a need for increased enforcement, parking in these areas was generally less than three hours, while the market car park attracted a high number of all day and medium duration parking, indicating a high use by traders/employees (approximately 40 spaces).

Other unrestricted parking areas had a mix of utilisation with parking on the south side of Forest Street being under-utilised.



Parallel parking with centre of road parking in Moyston Street west end.



**Opportunities**

- The proposed supermarket adjacent to the Market car park will generate additional demand for short-term parking in the area. The development will be reserving some parking for staff and providing additional on-site parking for customers. Some customers, however, will utilise the Market car park and adjacent on-street parking.

As part of the Market development, Council has received some funds to construct additional parking nearby.

It is recommended that these funds be utilised to increase on-street parking around Western Reserve as follows:

Wheeler Street:	10 spaces east side 12 spaces west side
Forest Street:	net increases 11 spaces (convert 11 parallel spaces to 21 angle parking spaces)

The angle parking on Forest Street would need to be indented by between 4 to 5 metres to ensure vehicles manoeuvring into and out of parking spaces do not obstruct the right hand through lane.

- The introduction of kerb outstands may provide some opportunity to increase the kerb side parking supply in Barker Street due to improved sight distance.  
As a consequence, parking can be continued to within 5 - 9 metres of the intersection of property boundaries, along Barker Street.

- As detailed in a second report for the Mount Alexander Shire Council by Andrew O'Brien and Associates Pty Ltd, titled 'Castlemaine Township Market Square Precinct Traffic Report' (January 1997), an opportunity also exists to increase the supply of parking in Hargraves Street. However, introducing centre of the road parking in Hargraves Street is likely to negatively impact its historic streetscape elements. This includes the steep open drains which make parallel parking unsuitable. Accordingly, given the other opportunities, the provision of the centre of the road parking is not recommended at this time.

- An opportunity may also exist to increase the parking supply in Frederick Street and Stoneman's car park by removing the eastern footpath along Frederick Street adjacent to the car park. Further investigation is recommended as part of any re-development of the old supermarket.

**Parking Periods**

The current 30-minute parking restrictions are generally appropriate for small centres with convenience goods. These restrictions are considered unsuitable to service the level of comparison shopping opportunities, which are available in Castlemaine. Comparison-shopping results in shoppers attending more than one store during their visit to purchase a range of goods and compare prices at different stores.



Wheeler Street could accommodate 22 parking spaces, if upgraded.



The need for longer-term parking was highlighted in the O'Brien Traffic Report where some over-staying was recorded. Accordingly, as previously recommended, it is considered appropriate that the 30 minute restrictions be progressively changed to 1 hour with the 90 minute restrictions increased to 2 hours. Some ¼ hour parking or ½ hour parking, however, may still be appropriate outside banks, video shops and other facilities, which primarily provide convenience shopping.

One half of the parking in the Market car park should be restricted to 2 or 3 hour parking, with the remaining half being similarly restricted once additional all-day parking is provided in Wheeler Street and Forest Street.

#### **Victory Park**

Victory Park, which is currently being up-graded, will be a significant visitor feature. With picnic facilities being provided, parking around the park should have a minimum restriction of 2 hours to encourage visitors to do some shopping during their stay.

#### **Castlemaine Railway Station**

The Castlemaine Railway Station, located on Kennedy Street near Templeton Street, contains a sizeable piece of under-utilised land. This land, sited between the station and the indoor cricket centre, could accommodate longer term parking for tourist buses and cars.

This item will need further investigation with tourist bus operators and VicRail.

#### **Western Reserve**

The section of Reserve off Barker Street south lends itself to use as a long-term carpark for cars and buses. Detail consideration is recommended along the lines illustrated on drawings 3 and 6. Access onto and from Barker Street would need to be subject to detail design and possibly Vic Road approval, given its use by slow turning vehicles.

#### **Parking Signs**

Parking signs, both restrictions and directional, should be progressively up-graded to the current Australian Standard symbolic signs. This up-grade should be coordinated with the increase in time restrictions recommended above. The symbolic signs are due to become mandatory. The use of meter parking within the CBD is not considered warranted at this time to control the use of days. Regular patrols should ensure that short-term restrictions are not significantly abused.

#### **Parking Recommendations**

To improve the supply and management of car parking in the Castlemaine CBD, the following recommendations are made:

- Replace parking restriction and direction signs with the current Australian Standard symbolic signs
- Increase 30 minute restrictions to 1 hour, providing some shorter term parking where needed, during the replacement of signs;
- Increase the 90 minute parking to 2 hours, during the replacement of signs;



Western Reserve off the southern end of Barker Street could be developed as a combination of long term carpark, picnic/playground and tree shaded irrigated lawns.



- Restrict 50% of the Market car park to 2 hour parking, with the remaining section to be restricted when additional all day parking is provided in Wheeler Street and Forest Street. Medium Priority

- Restrict parking across the frontages of Victory Park to a minimum of 2-hour parking. Low Priority
- Construct new angle parking along both sides of Wheeler Street. High Priority
- (Construct new angle parking in Forest Street outside Western Oval. Low Priority

- Maintain the existing parking configuration along Hargraves Street.
- Permit parking up to 9 metres from the intersections along Barker Street once kerb outstands are introduced. Low Priority

- Investigate the opportunity to increase the parking supply in Frederick Street/Stoneman's car park by removing the eastern footpath. Medium Priority
- Investigate the use of land within the Castlemaine Railway Station for longer term parking of long vehicles and additional all day carparking. Medium Priority

- We are advising against development of carparks behind shops and against the provision of rear entry to shops as this could contribute to a decline in the street presentation and vitality of the shopping strips.
- We are favouring short (30 mins) and medium (90 mins to 2hour) parking in most of the Town Centre area, with long term and all day parking at the perimeter of the town centre (ie Railway Station and Western Reserve)

- Consideration should be given to accommodation premises, without on site parking, being provided with a guest permit parking pass to allow longer parking adjacent to the B&B or hotel.

### 2.06 The Market Square and David's Supermarket Development

The new supermarket is likely to change the retail focus and place a demand on carparking in the locality. It will also effect occasional traffic delays until shoppers change habits to avoid peaks and spread the traffic and parking load over a longer period.

Whether the changes are detrimental to some retailers and beneficial to others will depend to some extent on how effectively the competing retailers can change emphasis and how effectively other traders and the community can support them. Council, with State Government funding will also undertake further streetscape works in Mostyn Street and new works in Barker Street to create as appealing a retail environment as possible.

Some Mostyn Street traders should gain immediate benefit from the additional numbers of shoppers in the locality especially those shops offering products and services not available in the supermarket. Those traders who are competing with the supermarket, such as the butchers and green grocers will need to be very competitive in style and price, also offering and presenting products attractive to tourists who are unlikely to visit the supermarket. This is a feature of South Australia's Barossa Valley



Retain existing carpark configuration in Hargraves Street and Mostyn Street.



towns where Butchers offer a big variety of processed meats, as well as regular cuts. Frequently they cook special sausages in front of the shop; the aroma is hard to resist; this practice is also enjoyed in Glenferrie Road, Hawthorn. Green grocers sell locally bottled grape juice and preserves, dried fruit and nuts with appealing packaging - some also concentrate on organic produce.

All traders need to work together to help each other present well and to offer an attractive complement or alternative to supermarket shopping. Coordinated signage, especially A frames, good quality cafe furniture, themed cafes (eg Hot Rod/Grease theme) and changes in product style could help a number of traders to attract a different custom.

**Mostyn Street East** is defining a style of craft trade, which could benefit from the increase in shopper numbers. It is likely that overflow parking will move into Mostyn Street East, Wheeler Street, and Forest Street and up Hargraves Street before other areas are sought out. This overflow will be of mixed benefit because some of those parking in front of the East Mostyn Street shops will only be interested in supermarket offerings while others may not otherwise have parked there and may take an interest in the shops they walk past.

**Barker Street North** could suffer most but it should present an appealing alternative to the new supermarket by offering a choice for those who do not find large supermarkets attractive and who appreciate easier parking options. The Riteway Supermarket is the key. If this store accepts the challenge, presents attractively and promotes itself as the logical alternative then other traders in the locality will benefit. It is in the interests of those other traders to encourage and assist Riteway to do so and to follow suit.

While the traders can do a lot to help themselves and each other, Council can match the effort by creating street and footpath conditions which are more attractive, more comfortable and safer (see detail sketches). Our recommendations are generally directed at traffic calming, street tree planting, pavement enhancement, street furniture and other measures to improve the appeal of the north end as the first priority for capital works.

## 2.07 Western Reserve

The reserve should be upgraded in a manner which visually draws it into the town rather than backyards it, as is the case at present. We are proposing a number of changes including a distinctive entry (see Appendix) to enhance its role as an entertainment/events and tourist park focus. At present the Midland Highway, Forest Street section, visually separates the reserve from the centre of town. However, this can be reversed by planting additional trees down both sides of the road and by replacing the post and rail barrier to the park, improving the entrance and the overall condition of the grass (include irrigation), pathways and furnishing.

With access/egress from Barker Street south of Forest Street, we propose the development of a long term parking area for shop staff, tour buses and for tourists cars especially those with caravans or trailers. Access is not suitable from Wheeler Street because the access road would severely compromise the oval and would be a hazard for those using the oval area.

As well as the carpark there would be a good level of lighting, a playground, upgraded picnic facilities and a good connection via pathway and pedestrian crossing to the Market Building and toilets. The carpark area off Barker Street would also function as a weekend market facility as well as a venue for car club rallies etc and could be designed for skateboarding or other multiple recreation uses such as netball, basketball or tennis. In this way, the community would ensure its frequent use and a greater sense of security.

Viewing pavilion and change rooms with toilets could be added should the demand be there. The Reserve also lends itself to exhibition of contemporary sculpture and should generally be attractive to young people.



**top:** The supermarket hall was designed after the Market Building style, although the proportions of the new building are greater overall.  
**centre:** Maintaining the old school frontage to Mostyn Street is a significant feature of the new supermarket, but it is also a significant example of the Council's determination to conserve its heritage.  
**bottom:** Western Reserve can be upgraded to become a major town centre asset.





Chapter 3

RETAIL IMAGE & TOURISM





CHAPTER 3: RETAIL IMAGE AND TOURISM

3.01 Strengths, Weaknesses, Opportunities and Threats

When contemplating Castlemaine's future as a service and tourism centre, it is helpful to identify its strengths, its weaknesses, the opportunities and the threats (real or potential). The following is not an exhaustive list but it offers both encouragement and challenge. There is no basis for despondency or despair. Where possible, the strategy is recommending steps to deal with the threats and to encourage exploitation of opportunities. The opportunities form the basis of challenge and with the continual encouragement of young people to become involved in imaginative commercial activity and in enlivening established commerce, Castlemaine can look forward to a good future. Castlemaine's town centre and its immediate regional environment has enviable strengths which should help its commercial area to maintain its viability. Acceptance of the challenge is part of the vision.

*The strengths are:*

- Most retail needs are present in the town
- Substantial food processing and manufacturing enterprises providing stable employment
- Many government and local government services are available
- The major banks and other commercial services are present
- A major supermarket ( this is also a threat to some traders and an exploitable opportunity for others)
- A strong historic visual character and pleasant ambience
- Strong regional tourist attractions and the Diggings project
- Restored Market Building.
- An even distribution of commercial and institutional activity reducing the concentration of traffic and parking.
- Passing trade on the Midland and Pyrenees Highways
- The regional education facilities, both government and private
- The court, Police station and legal services
- Very good Hospital, Medical and Community Health services
- Rail service
- Castlemaine Gaol with its convention facilities
- The Art Gallery and Museum
- The Theatre Royal
- A good standard of cafe options
- A small but significant range of craft products
- Considerable artistic talent

*Weaknesses are:*

- lack of community support for local businesses
- inadequate turnover in some businesses to allow competitive pricing and holding of an extensive range of stock
- lack of emphasis on young people for food outlets and clothing
- over supply of some businesses eg hotels and outdated take away food outlets
- poor presentation of some businesses
- poor trader attitude of some businesses

Goods on display in shopping streets is now a common practice and adds to the colour and interest of the commercial area.





- Threats are:**
- Impact on Barker Street traders of traffic on the Midland Highway
  - absence of a cohesive and interesting Castlemaine image or product marketing
  - lack of cohesive Castlemaine imagery in retail signage
  - insufficient shaded parking
  - age and condition of some retail premises
  - insufficient, quality street furniture
  - poor window displays
  - inadequate use of display lighting
  - Lack of interest in upgrading personal and produce presentation
  - insufficient tourism biased products in the low to middle cost range
  - inadequate picnic and parking facilities for tourists
  - retail staff using parking areas required for shoppers

**Threats are:**

- The continued loss of trade to Bendigo
- Possible future upgrade of Midland Highway to detriment of Barker Street
- Some traffic using Kennedy Street and Hargraves Street as by pass options
- New supermarket creating a retailing location bias and effecting decline of some small competing businesses offering the same products without the margins for competitive discounting.
- decline in regional tourism and a change in tourist needs and interests

**Opportunities are:**

- To exploit the gold history of Castlemaine and the Diggings project
- To exploit the presence of a major retail food outlet by promoting complementary retailing
- A Castlemaine/Maldon steam train service and exploiting the presence of train travellers. Need for information service at the station.
- Upgrading and representing some takeaway food outlets to appeal to young people
- exploiting the night time appeal of Castlemaine
- exploiting curiosities to encourage walking of the town centre
- Consolidation of craft product retailers and antique dealers
- Greater emphasis on local produce as part of tourism marketing
- Greater specialisation and niche market development especially in the hotel trade. Specialist wine bars or Apple cider bars or night club/coffee lounges, specialised themes such as steam railway, hot rods, gold, Cobb & Co
- to proactively encourage the Barker Street North shopping area, centring on the Rileway supermarket, as a second option to the David's supermarket precinct.
- Greater involvement of young people in all areas of commercial activity, services and tourism
- Expression and animation of Castlemaine history by retailers
- greater emphasis on appealing to young people via products and their presentation
- Selling Castlemaine's commercial strengths to entice new businesses not currently in the town
- improved long term and tourist parking options
- improved appearance and facilities in Western Reserve to encourage its use for events and weekend markets
- introducing traffic calming measures and making other improvements to Barker Street to promote its image as a shopping street
- enhancing the image of the Civic precinct by lighting and appropriate street furniture
- better planning of shop opening hours to reflect experience and the needs of niche markets
- night time opening of the Art Gallery/Museum during tourist peaks.



No longer a threat, but Castlemaine has economic threats.



### 3.02 Retail Strategies

There is no single strategy that will help to revitalise the town centre, there are however, many fronts on which the trading community and Council need to work. Some considerable efforts have already been made to assist this cause including the new Supermarket and the restoration of the Market Building.

The new supermarket development is expected to be an important part of Castlemaine's commercial survival. It is anticipated that new shoppers will be attracted to Castlemaine and there is a hoped for reduction in the number and frequency of shopping trips to Bendigo. Nonetheless, the whole of Castlemaine's commercial area needs to be more enticing, especially to young people.

Castlemaine must compete with Bendigo as an attractive shopping and tourist destination. Unless the need to compete is adopted as an imperative by all, not just some traders, the supermarket will not be enough to revitalise the entire strip shopping areas. Traders must give more attention to detail and to the overall appeal of their shops and cafes and ask whether they are missing out on a significant market, especially the young people. By not catering for young people there are many more shopping and recreation trips to Bendigo than would be the case if Castlemaine offered an attractive alternative. There is clearly a need to re present the take away food trade. Consider mergers and definitely take a leaf out of the book on multi national take away food retailing in terms of cleanliness, efficiency, image and young person appeal - otherwise, sooner or later the big boys will move in.

One of the most effective ways to establish and maintain commercial appeal is for those who are in contact with shoppers to be outgoing, considerate and supportive toward patrons and fellow traders. This is free and helps to establish a critical mass of like-minded traders who, as a team, could make shopping in Castlemaine a sheer delight.

Aussie Host offers a tremendous opportunity to improve personal image and opportunities to help and inspire young people should be taken advantage of.

There are however, some practical problems in the town that has an effect on trade irrespective of the trader's efforts to make themselves and their shops appealing. *If* it is difficult to park safely, *if* cars are in the hot sun, *if* there are too few reasons to shop in a particular locality, or *if* there are insufficient reasons to be in that part of town then it will may be neglected.

While Council, with State Government funding can help to lessen these problems by making a contribution to the street, they cannot do all that is needed to make Castlemaine commercially vibrant. Ways in which Council, with State Government support can help the CBA include -

#### **Barker Street:**

- Changing the perception of Barker Street from 'Highway' to shopping street. Measures include introducing a roundabout at the intersection of Campbell Street and Barker Street, constructing pedestrian outstands at the corners of each intersection on Barker Street; planting additional trees in the parking lanes (without reducing numbers of parking bays), regassing, irrigating and extra tree planting within the central median and by introducing extra street furniture, flowers and lighting.
- The roundabout would be a significant shopping area entry statement as well as being a traffic-calming device.
- The southern end of Barker Street (south of Forest Street) would also be improved by; at the southern most extremity, regassing, regrading, grassing and Oak Tree planting the area just east of the railway underpass. In addition, plant Oaks on the west side of the underpass; planting Prunus in



Attractive, well lit window displays add to the town's interest by day and night.





Interpretation of major tourist attractions would add to the visitors' knowledge of Castlemaine and generate a greater degree of interest. This Burke and Wills monument is the most important opportunity for interpretation.

*Geoff Sanderson & Associates*

*Mostyn Street* has already gained from the restoration of the Market Building and it will further benefit from the Market Square development, there is also proposed tree planting in the centre median and additional street furniture recommended.

The key aspects of trader improvement are listed in "opportunities" above.

The appendix also includes examples of inexpensive but successful window display, signage and lighting which, properly and attractively implemented will all help to lift the overall image of the town centre.

### 3.03 Tourism Strategies

#### *Background and Tourism Data*

Tourism is an inseparable part of Castlemaine's economy but it is important that the interests of tourism not dominate the physical character of the town and cause the town to appear as a tourism contrivance. It is the lived in quality of the town and its matter of fact ability to live amongst its heritage gems without compromising them that provides the real attraction for the tourist.

There are some important tourist resources which have economic benefit for the town, such as the old gaol and Buda and these need to be supported but there is a general lack of things for tourists to buy and things for them to do within the town centre.

The following should help to give a clearer picture of regional tourism and a better understanding of what Castlemaine needs to do to take greater advantage of the number of visitors who pass through.

The Tourism Victoria (1996), Victorian Regional Travel and Tourism Survey, 1995 document has been a useful source for the analysis and information that follows. AGB McNair Pty Ltd on behalf of Tourism Victoria carried it out. The survey was funded from a grant provided by the Victorian Government Community Support Fund.

The summary document, August 1996 says "The survey presents an overall picture of regional tourism throughout Victoria. The results will be of use to tourism, operators, industry, investors, developers, consultants, students and Government." Although the survey was conducted in 1995 the findings are still current.

Regions and sub-regions have been defined and linked together. For the purposes of the Castlemaine Study the relevant region is Goldfields comprising three sub-regions including the Macedon Ranges and Spa Country, Ballarat & Surrounds and Bendigo/Loddon areas. Each of these sub-regions is physically grouped together in the central area of Victoria. Castlemaine therefore falls within the Goldfields Region and the Bendigo/Loddon sub-region for the purposes of this study.

The Goldfields region attracts in the vicinity of 1.7 million visitors per annum (AGB McNair 1995) and for the Bendigo/Loddon sub region, the average length of trips was 2.5 nights. The largest proportion of visitors came from Victoria and most of those from Melbourne.

The survey states that of the overnight visitors to Victoria during 1995, 75% visited regions outside Melbourne. Over 5 million visitors travelled to the Melbourne region, 2.2 million visited the Great Ocean Road and 1.7 million travelled to the Goldfields. It is significant this region was the third most visited (by 1.7 million people) particularly for any future development and tourist initiatives.

Maintenance or improvements to this position (third highest) would be desirable through the



encouragement of tourist facilities and this location as a desirable destination. The design guidelines will need to consider this aspect of future development.

A high proportion (76%) of visitors to the Goldfields region were visitors from Victoria with these visitors more likely to stay with friends and relatives (49%). The main purpose of visiting the region was specified as for pleasure or holiday (44%) and staying with friends or relatives (29%) which was consistent with the whole of Victoria.

Parties of three to five members (47%) or two members (36%) made Day trips, most of which could be adults with children. The report also suggests that the Bendigo/Loddon sub-region generated the largest number of day trips for this region with 52% of all day trips taken in the region.

Consistent with this aspect was the party composition and the main destination. For the Goldfields region, adults with no children were more likely to visit the Spa Country (84.1%) than the Bendigo/Loddon sub-region (79.7%) and Ballarat & Surrounds (73.4%), than adults with children who were most likely to visit Ballarat & Surrounds (25%) than Bendigo/Loddon (19.6%) and Spa Country (14.5%).

One reason why adults with children may prefer to visit Ballarat & Surrounds sub-region is the opportunity to visit Sovereign Hill in Ballarat. This reason is supported by the following details.

Day trips by visitors to the various sub-regions generated a diverse range of activities. Of significance to the Bendigo/Loddon sub-region is the number of visitors (11%) who went to the Ballarat & Surrounds sub-region for the purposes of visiting a theme park or amusement park or historic park. Compared with 0.6% Bendigo/Loddon.

Visitors to the Bendigo/Loddon sub-region were more likely to visit friends and relatives, and go shopping than other activities and also more than for the average of the region.

**Comment:**

White Castlemaine has some excellent venues especially in the food sector and some second hand and craft outlets (eg glass blowing), the town centre is not as great an attraction for tourists or repeats as it could be. The Gaol and Buda work well but within the town centre there is not enough to enthuse the visitor or encourage them to stop and spend.

Clearly one asset that the Ballarat & Surrounds sub-region has is the Sovereign Hill Thematic/Historic Park. Sovereign Hill is the principal regional attraction for young people as well as all other age groups. It is well patronised by international, interstate and intrastate visitors and is a key reason for visiting the region and more specifically visiting Ballarat & Surrounds sub-region. Castlemaine still appeals to adults but it needs to seriously consider providing greater emphasis on the interests of young people. It is interesting to note that the top ten tourist attractions in Australia are all entertainment related, even the Rocks area of Sydney as number 8, has an overwhelming food and entertainment character. Apart from Casinos and the Sydney Opera House, all other attractions cater for young people.

Without compromising the Castlemaine town character, it should be possible to develop a few youth drawcards, such as a top class "Grease" cafe which would help to give Castlemaine a regional market advantage.

It is important to have a steady flow of tourists and repeat tourists of all ages, they are the bread and butter market. It is also important that they spend, not only on basic food and accommodation but also

Lealy dignity – hallmarks of Castlemaine.





on other produce, art, services and bric a brac. The greater the range of items which are peculiar to Castlemaine the better. There is a tendency nonetheless, for tourists to look but not spend, except on food and entertainment. Castlemaine has the food, although not much to take home, but it has little in the way of entertainment.

Another key is to make Castlemaine inviting to the big range of special interest groups including car clubs, cyclists, bush walkers, education groups, convention groups, garden clubs, sporting clubs, artists, musicians, gold seekers, media, historic societies, hot rodders, wine buffs and any others that spring to mind. Simple research will identify the special needs of each and the efforts Castlemaine would need to make to accommodate them.

*There are many questions to be asked about the capacity of Castlemaine's town centre to take advantage of the number of tourists who visit or pass through.*

**Some of the questions which need to be asked are:**

While Tourists can rarely resist the temptation to shop, what can they buy?

- Is there anywhere that you can buy anything to do with gold prospecting, 19<sup>th</sup> Century packaged products?
- Is there any specialist, old-fashioned shops selling Castlemaine small goods?
- Is there a specialist delicatessen selling Castlemaine small goods?
- Is there a bakery selling 1850's goldfields breads, cakes, pies and damper and promoting it?
- Is there a commercial gallery providing a sales outlet for local painters and sculptors?
- There is a shop selling old-fashioned clothes but how often is it open?
- Is there a cafe with a goldfields theme including the cakes, breads and beverages sold in the 1850's to 1860's?
- Is there a cafe with the hot rod/Grease theme?
- Is there a printer selling original newsprint and photographs to celebrate the goldfields history and events such as Burke & Wills travelling through Castlemaine or the first gold shipment or the build up to the Eureka stockade (who is aware of Castlemaine's involvement)?

**What can tourists do, what can they find out about Castlemaine and how well is it marketed?**

- How many visitors know about the museum display of Mt Alexander Diggings and the significance of the Art Gallery and its collection?
- How many buildings and places can be visited where the history of those buildings and places is told?
- Where are the original Cobb & Co stables, where is the Cobb & Co story told?
- Is there a working display of the different forms of gold mining?
- Is the Burke and Wills monument well interpreted and their story and the relevance to Castlemaine told.
- When passengers arrive at Castlemaine Station, how much information is available at the station? If the Market Building was a market, why can't it still be a market for high quality craft? Surely the Market building should offer attractions such as craft workshops and sales.
- When Mostyn Street is busy, where do the tourists who are drawn to the Market Building, park? Are there volunteer or commercial conducted tours of Castlemaine?



The Midland Private Hotel offers a unique blend of Victorian and Art Deco architecture.



**What about the kids?**

What is there within the Town Centre that would interest kids? Remember that 1.7 million people visit the Goldfields region every year and at least half will make decisions on where they go based on the interests of kids. Sovereign Hill wins easily.  
Naracoorte for example, has a wool museum with an entire floor built to the scale and tactile curiosity of children.  
An old-fashioned (1950's) "Grease" cafe/milk bar could be a winner?  
Working machinery and Hot Rods within the town would also appeal.  
What if it was possible to describe the size of the pile of gold that was extracted from Castlemaine eg "imagine the Western Reserve 1 meter deep in gold"  
Horsham and Bendigo are two cities with well known and well placed parks and playgrounds for the weary traveller.  
Western Reserve could well provide such a facility if it had a good playground, more picnic facilities and a well maintained toilet block (the toilets within the Market Square precinct may not be obvious or convenient for the highway traveller and the playground cannot be easily seen from the highway)

**What else?**

- How many shop windows are of interest to the tourist?
- Could the shoe shop feature some of the footwear typical of the workers, the military officers, the ladies and the well to do of the 1850's to 1860's?
- Could the Criterion tell the story of Cobb & Co.?
- Could the banks tell us about the amount of gold won in the Mount Alexander diggings and tell us what it would be worth today? Framed stories in the bank windows along with some of the trappings of gold handling such as scales, calico bags etc and the stories of attempted hold ups would be a great drawcard and good for the image of the particular premises.
- Could a dress shop feature a glamorous display model showing a fashionable dress of the 1860's?
- Could the supermarket window have a framed collection of produce advertising and a description of a typical day's food for the gold diggers of varying nationality?
- Could another pub tell us about the Castlemaine Brewery and what the X's are all about and how much grog was consumed by the miners and the various tall and true tales of pub brawls etc.?
- Could the Council office establish a notice board outside the Municipal Office with extracts from the very first years of Council?
- Could the Police Station or the Court House do the same and display posters of some of the bushrangers and other villains of the gold field days?

None of the above needs to be presented in anything but the most subtle, well-crafted manner otherwise the town could quickly become the tourist tinsel town we are all trying to avoid.



For window displays to work they need to be interesting and attractive. The back of the display shelf is neither of these.



Chapter 4

DESIGN ISSUES



top left: A well presented bookshop  
top right: A sad bush – obscuring a fine building.  
bottom: This shelter is to be removed, when it goes, the  
quality of the locality will improve measurably.



CHAPTER 4: DESIGN ISSUES

4.01 Future Streetscape Character

As a historic town, Castlemaine's economy benefits from tourist spending but of equal importance is the influence the town's character has on community lifestyle and on its regional appeal as a shopping destination. Conserving the historic character is paramount to the Castlemaine vision and the whole flavour of our recommendations is to this end. We do not believe however, that such emphasis need compromise the liveability of the town.

The Castlemaine community needs to be clear about what the town should look like, should its street furniture be much the same as it had in the 1860's to 1930's or should it bring in new ideas? Our approach is to retain what belongs in Castlemaine and resist the temptation to import any of the vast array of furnishings found in Melbourne, or Ballarat or even Bendigo. Castlemaine has the resources to make what ever is needed.

Some of the historic details are found in other places but, to conserve what is distinctively Castlemaine we should encourage use of these traditional details where they are appropriate, rather than use imported furnishings and other details which could progressively cause the town to look like somewhere else.

This strategy aims to prepare Castlemaine for its future; it may challenge the people of Castlemaine to think about what they have now and what they still wish to have in the foreseeable future. Change of any kind can bring with it trends, ideas, colours and materials which can quickly dominate the townscape. This is not a problem in many towns, but in Castlemaine it is because the town economy depends to a large extent on keeping its historic character intact. Therefore, most of the details we are recommending are historically authentic and can be reproduced from historic photographs.

Street furniture is as relevant as heritage buildings to the overall character of the town and so it is not valid to simply select such furnishings from a catalogue or by admiring a seat or a litterbin in another town. The same applies to colour, Castlemaine should derive its details and its colours from those that were commonly used in the town at the time most of the town was built.

We depart from historically authentic detail only to accommodate necessary changes to traffic and pedestrian movement and to accommodate works of art however, we recommend resisting details which conflict with the town's heritage or which compete with the scale and colour of heritage buildings.

The township is easy to walk and walking should be encouraged as a way to properly absorb the collection of large and small interests. If stories can be told and if what is seen can be explained, the walk could be much more rewarding. For example, if Eaglehawk linked a place to the story of Muga Bill's Bicycle and his disastrous ending, tourists would have a special reason for stopping in Eaglehawk. Castlemaine should install a plaque near the Burke and Willis monument that gave a summary of the Burke & Willis story and explained why the monument was there. Tourists, especially foreign tourists, would then have a better understanding of Australian history as well as Captain Burke and Castlemaine's role in the ill fated expedition.

In general we recommend replacement of any inappropriate street furniture and for the streetscape advisory committee to recommend locations for additional furniture in consultation with traders where necessary.

The rail bridge at the southern end of the Midland Highway entrance is a fundamental aspect of Castlemaine and must be conserved.



Simple, traditional street furniture and stately trees are part of the Castlemaine culture.



#### 4.02 The role of Artists

Castlemaine has a strong, talented art community, which is under utilised in the public forum. There is a great opportunity for Castlemaine to use their talents in the expression and animation of the town's 19<sup>th</sup> Century history as well as reflect on the contemporary.

The Shire's Cultural Mapping project will provide a marvellous resource in this regard and there are many applications for the artists' skills.

The following are some of the ways in which artists could embellish Castlemaine:

- to develop pavement inserts on outstands at the main intersections and as welcome mats for shops - each one different according to the whims of the trader and the specific history of the shop.
- to create small enamelled plaques which tell stories, cross reference to tour guide brochures and fixed to shop fronts or inside shop windows.
- to create metal silhouette or similar sculptures and shadow patterns to be judiciously placed where they can help to tell a story.
- to paint a tree surround picket, one for each tree surround
- to produce framed histories of shops to put in shop windows so that passers by can read them.
- to create sculptures for public places

Other artists including musicians and actors should be given encouragement to develop Castlemaine events and establish a Castlemaine tradition for high quality productions in a range of music and live theatre appropriate to available skills. While there may be some benefits in Castlemaine being a venue for State productions, there is greater benefit in developing a well supported local reputation.

#### 4.03 Street Furniture

The Appendix illustrates our recommendations concerning all street furniture. In summary, we recommend resisting the proliferation of cast iron bollards and sign posts and the reintroduction of the timber protective barriers and bollards at street corners to protect stone lined drains and pedestrians on outstands.

Only adjacent to civic buildings should cast iron bollards be used and these should be more substantial (eg, like those in front of the Botanical Garden) than the thin style that has been used in recent times.

Seats should be of the traditional style that were used in Castlemaine in the 19<sup>th</sup> Century and is still evident in the town (see appendix).

Litterbins were not necessary in the 19<sup>th</sup> Century and were not used until the 1950's. The style of bin recommended have already been used in some parts of town and we recommend all older bins be replaced with this type as illustrated in the Appendix.

- **Signs** - are a special item and one that can have a big impact on the town character. Our recommendations are to:
  - develop a Castlemaine logo for pedestrian level signs and use it on a common style of A frame and under verandah commercial signs ( see detail sketches).
  - reintroduce the black and white cast signs for street names and for pedestrian directional signs. Pedestrian directional signs should not try to attract motorists but should be of a scale and in a position where pedestrians can comfortably stand and read them. These signs should be fixed to hardwood, painted timber posts again of traditional Castlemaine style (as illustrated in the Appendix)
  - Vehicle direction signs need to be clear, recognisable (eg Police logo) easily read and has limited information. Chapter 2 Traffic and Carparking makes a clear recommendation in this regard.



Unattractive street furniture is often an unwitting deterrent to shoppers. The same places with appealing furniture, signs and window displays.



**4.04 Pavement and Drainage**

We recommend (as illustrated in Appendix)

- New or replacement pedestrian pavement be either standard bitumen or precast concrete units of similar size, pattern and colour to the original slate pavements (as still exist outside the Post Office in Lytleton Street and similar to the precast concrete pavement in Mostyn Street, east end - north side. Specific inlays or trims in red clay brick on edge, or in sawn, exfoliated granite or mosaic art works, or brass lettering, would be embellishments in keeping with the town's vision.
- Any future kerb and gutters should be made of Harcourt granite (or precast, sand blasted concrete imitations) and all existing stone lined drains and kerbs to be cleaned and conserved.
- Existing sections of streets with unpaved, gravel verges should be maintained in that style however, gravel should be similar to the apricot coloured material used at the entrance to the Botanical Garden.

**4.05 Lighting**

Effective lighting can give another dimension to Castlemaine, adding significantly to its attractiveness for residents and visitors alike.

Some buildings are lit at present, such as the Municipal Building and the Old Gaol however, there are few others and even fewer shop windows, giving little reason to walk the town in the evening. All statues especially Burke and Wills and the war memorials, Patterson's statue and the proposed statue in the centre of the new Hargraves Street/Forest Street roundabout need to be properly lit. Many buildings could be lit with existing lamps eg the Midland Hotel, the Art Gallery and Museum facade. The Post Office is very attractive when its entry verandah lights are on; the railway station and railway sheds should be properly lit, the avenue of Golden Poplars on the Midland Highway could be lit in Autumn, the Court House verandah lights should also be left on.

Commercial building owners could improve lighting under verandahs by installing lamps eg bracket lamps fixed to the facade and to leave them on at night. Window display lighting using 12-volt lamps would be inexpensive and would add to the overall interest of the street at night.

Street lighting should be increased in a few locations eg in Templeton Street however, in general, the best effect can be gained by display and special effect lighting which is inexpensive and could stimulate greater use of the town at night.

**4.06 Trees and other Planting**

We recommend retaining the established tree species as the dominant members of the long-term trend character of Castlemaine. We do this not only because the Oriental Planes have thrived but also because they are now a strong part of the Castlemaine vernacular. Castlemaine has some very fine Oriental Plane Trees, many of which are well over 100 years old, and some are around 130 years.

There are also some Elms, Cotton Palms, Canary Island Palms, English Oaks, Lombardy Poplars, Golden Poplars and Silver Poplars of similar age. It is probable that the botanist and Director of the Melbourne Botanic Garden, Baron Von Mueller, who was a great fan of the Oriental Plane Tree influenced the early planting of Castlemaine and it is also possible that the plants came from the Melbourne Botanical Garden nursery. In the 1960's, Prunus, Claret Ash and Prickly Paper Barks were fashionable and Castlemaine has its share too.



**top left:** A small lamp left on at night would add to the charm, interest and safety of this space.  
**top and bottom right:** The stone lined drains are a priceless, cohesive streetscape element and need to be conserved.  
**bottom left:** Well lit shops add much to the town's night time appeal.



There are also some Sugar Gums and Yellow Gums, the former probably planted around the 1930's and the latter in the 1960's.

**Tree planting recommendations are:**

- that the Oriental Plane Tree is further planted in the Town Centre streets eg Lytleton Street east end and the central median between Hargraves Street and Barkers Street, and that they replace the Prickly Paper Barks in Campbell Street, Barker Street and Mostyn Street also have some additional plantings of Oriental Plane Trees.

- We do not advise continued planting of the Cut Leaf Plane, as there are already a significant number and they depart from the original character.

- We also do not recommend further planting of Elm species due to the risk of Dutch Elm Disease.

- In Mostyn Street west end and Lytleton Street west end we recommend Hackberry Trees to provide shade for parked cars. They are a smaller, tough tree which will not dominate the Palms, will handle dry conditions and provide good shade. The Canary Island Palms would remain dominant in the centre of both streets at the west end.

- There are a number of English Oaks in the town and although they may be slow to start, they are important to retain within the townscape. Accordingly we recommend planting them at the southern, Midland Highway entrance to the town.

- We also recommend some additional planting of Cotton Palms and Prunus in Barker Street median and other Cotton Palms beside the railway station and the railway shed. In the latter locations they would help to frame the buildings without obscuring them as happens with the present planting or would happen with any other tree species. Prunus Trees were planted in the 1950's and 60's, they add spring colour and should be retained. Additional Prunus planting would only be to reinforce an established character eg in Barker Street median.

- White Cedar Trees are proposed for Forest Street west end.
- Claret Ash are attractive however, they too are at some risk of disease. We have only proposed a small number of additional Claret Ash where it is worthwhile consolidated the existing effect eg South end of Barker Street.

- There is room for further planting of Golden Poplars in Western Reserve to add their rich gold colour to the township.

- In Mostyn Street, east end we do not propose any significant new planting however, should the Yellow Gums decline, they could be replaced with Yellow Box or with White Cedar Trees if deciduous species are preferred. We believe this section of Mostyn Street should however, maintain its distinctive native tree character.

An essential aspect of street tree planting is their nursery production, planting preparation and protection. We recommend selecting trees grown in Root Control Bags and Spring Ring containers in order to reduce the potential for invasive and aggressive root development that can damage pavement. We also advise proper ground preparation to further reduce the potential for poor plant growth.



top: Celtis australis (Hackberry) is recommended as shade trees for the Mostyn Street west and Lytleton Street west footpaths to shade parked cars.  
centre right/bottom right: The Oriental Plane Tree is recommended as the dominant street tree.  
centre left/bottom left: There is a place for Poplars in Western Reserve where their size and colour can be appreciated.





*Other planting of shrubs, ground covers and annals in traffic islands should emphasise plants which were commonly grown in the 19<sup>th</sup> Century rather than contemporary hybrids such as bedding roses or formal hedge plants such as Box (although they are appropriate in other locations).*

Shrubs and perennials such as Agapanthus, Rosemary, English Lavender, Pelargonium, Cerastium, Aster, and Annals such as Phlox, Zinnia, Nemesia and Antirrhinum are in keeping with the town's character.

All tree and other planting needs proper care and maintenance without which they cannot be expected to reach their potential. Any funding for planting should have a commitment of funds for maintenance determined at the same time and specific to the new planting.

#### 4.07 Statues and icons

The principal issue is that none of the statues and special places of interest are successfully

interpreted at the location or in a comprehensive tour folio of Castlemaine. There is a piecemeal

approach to their explanation and some are not well displayed due to parking dictates.

Each existing sculpture and icon needs to be examined, repaired, interpreted, lit and given an

appropriate setting.

Consideration should also be given to a continuation of the tradition for commissioning and placing

sculpture to commemorate great forthcoming events eg 150 years since gold discovery.



The domed Romanesque towers on the Market Building have a strong relationship with Cotton Palm. In concert they are one of Castlemaine's greatest icons.



## Chapter 5

### IMPLEMENTATION



CHAPTER 5 IMPLEMENTATION

5.01 Staging and Priorities for Capital Works

**Stage 1** works are to begin during the 1997/98 financial year and would focus on Barker Street north. The priority is given to this locality in view of the possible effect on trade of the new David's supermarket in Mostyn Street. Upgrading the streetscape and introducing traffic calming measures can help to offset this effect.

*Works should include*

- kerb outstands and splitter islands for the Barker Street/Templeton Street intersection, and kerb outstands at the existing pedestrian crossing;
- upgrade of the central median from Campbell Street to Forest Street, including regassing, irrigation and planting of additional Prunus Trees and Cotton Palms.
- Supply and planting of fifteen Oriental Plane Trees (with picket surrounds) to be planted on both sides of Barker Street in accordance with the urban design plans.

**Stage 2** works should continue to focus on Barker Street as well as begin the redevelopment of Western Reserve, an important aspect of the Pyrenees Highway entrance.

*Works would include*

- The Barker Street/Campbell Street roundabout
- The remainder of the Barker Street kerb outstands at the Lytleton Street and Mostyn Street intersections
- The new post and rail fence and entry gate to Western Reserve
- Indented carparking in Forest Street and tree planting in Forest Street
- Formalised parking and tree planting in Wheeler Street
- Regrade the area of land at the southern end of Barker Street, grass and plant with advanced English Oaks.

**Stage 3** works should finish any outstanding or incomplete works on Barker Street and Forest Street, complete the redevelopment of Western Reserve and undertake works on Hargraves Street.

*Works would include*

- Completing tree planting and median upgrade for southern end of Barker Street including any works to facilitate access and egress re Western Reserve carpark.
- Complete Western Reserve works including the pathway construction, upgrade of oval, construction of carpark and playground, tree planting and park furnishing.
- Construct kerb outstands and splitter islands at Mostyn Street/Hargraves Street intersection and at the Forest Street/Barker Street intersection.
- Reconstruct Lytleton Street/Hargraves Street outstands and splitter islands
- Construct kerb outstands and splitter persons crossing in Hargraves Street
- Tree planting in Templeton Street, Lytleton Street, Mostyn Street and Hargraves Street

**Stage 4**

*Works should address*

- The Railway Station carpark and landscape works,
- Any remaining tree planting including Campbell Street, and
- Items including feature lighting, street furniture,
- the Campbell Street/Barker Street roundabout sculpture
- any other minor works not covered by stages 1 to 3 or deferred from stages 2 and 3.
- The proposed pavilion and toilets for Western Reserve could also be considered at this stage.



5.02 Costing schedule  
 The cost schedule was prepared in May 1988 for budget purposes only and as a guide for determining budgets for different combinations of projects. The rates will vary depending on ultimate construction details, packaging of works into cost effective projects and on variations in pricing structure over time.

Geoff Suckerman & Associates

Item	unit	qty.	cost/unit	total
STAGE ONE				
TR EES - supply + plant	no.	2	\$ 500.00	\$ 1,000.00
Cotton Palm	no.	15	\$ 500.00	\$ 7,500.00
Plane Trees	no.	7	\$ 450.00	\$ 3,150.00
Prunus supply and plant	no.	82	\$ 20.00	\$ 1,640.00
Flower beds, excavate, soil and plant	m2	320	\$ 18.00	\$ 5,760.00
Bituminous concrete pedestrian pavement	ll.m	130	\$ 42.00	\$ 5,460.00
Sandblasted concrete kerb + channel	no.	23	\$ 80.00	\$ 1,840.00
Timber bollards + chains	allow			\$ 3,000.00
OTHER HARDWORKS				
Road Pavement demolition and making good	ll.m	75	\$ 42.00	\$ 3,150.00
Sandblasted concrete edging to traffic islands	no.	15	\$ 300.00	\$ 4,500.00
Timber tree guards - supply + installation	item		\$ 15,000.00	\$ 15,000.00
irrigation	item			\$ 63,140.00
Sub total with contingency		10%		
STAGE TWO				
TR EES - supply + plant	no.	10	\$ 450.00	\$ 4,500.00
Claret Ash	no.	5	\$ 450.00	\$ 2,250.00
English Oaks	no.	29	\$ 500.00	\$ 14,500.00
Plane Trees	no.	13	\$ 450.00	\$ 5,850.00
Prunus	no.	15	\$ 450.00	\$ 6,750.00
White Cedar	no.	133	\$ 20.00	\$ 2,660.00
Flower beds, excavate, soil and plant	m2	410	\$ 18.00	\$ 7,380.00
Bituminous concrete with sub base	ll.m	145	\$ 42.00	\$ 6,090.00
Sandblasted concrete kerb + channel	no.	44	\$ 60.00	\$ 2,640.00
Timber bollards + chains	m2	21	\$ 10.00	\$ 210.00
OTHER HARDWORKS - supply + installation	item			\$ 3,948.00
Insitu sand blasted concrete edgings	ll.m	94	\$ 42.00	\$ 3,948.00
Extended carparking for Forest and Wheeler	m2	675	\$ 18.00	\$ 12,150.00
Streets (bitumen surface) - supply + installation	no.	43	\$ 100.00	\$ 4,300.00
Concrete wheel stops for Forest and Wheeler Streets	item			\$ 75,000.00
MISCELLANEOUS				
Design/documentation fees	item			\$ 16,000.00
Timber tree guards - supply + installation	no.	54	\$ 300.00	\$ 16,200.00
Post & rail fencing - supply + installation	ll.m	300	\$ 30.00	\$ 9,000.00
Western Oval Reserve entry from Forest Street - design + construction	item			\$ 5,000.00
Regrading + grassing at the southern end of Barker Street	item			\$ 7,000.00
Sub total with 10% contingency		10%		\$221,570

Item	unit	qty.	cost/unit	total
STAGE THREE				
TR EES - supply + plant	no.	1	\$ 450.00	\$ 450.00
Claret Ash	no.	58	\$ 450.00	\$ 26,100.00
Celtis	no.	7	\$ 50.00	\$ 350.00
Eucalyptus viminalis	no.	77	\$ 500.00	\$ 38,500.00
Plane Trees	no.	68	\$ 450.00	\$ 30,600.00
Poplar Trees	no.	19	\$ 250.00	\$ 4,750.00
Prunus	no.	7	\$ 50.00	\$ 350.00
Yellow Box	no.	17	\$ 10.00	\$ 170.00
Flower Beds	m2	525	\$ 18.00	\$ 9,450.00
Bituminous concrete with subbase	ll.m	335	\$ 42.00	\$ 14,070.00
Sandblasted concrete kerb + channel	no.	61	\$ 60.00	\$ 3,660.00
Timber bollards + chains	ll.m	14	\$ 42.00	\$ 588.00
Sand blasted concrete edging	item	30	\$ 35.00	\$ 1,050.00
Access/Egress to Western Oval Reserve carpark	m2	1170	\$ 10.00	\$ 11,700.00
Apricot coloured gravel path (1.8m. wide)	m2	200	\$ 6.00	\$ 1,200.00
Carpark gravel areas	m2	3050	\$ 15.00	\$ 45,750.00
Bitumen carpark surface	ll.m	740	\$ 32.00	\$ 23,680.00
MISCELLANEOUS				
Design/documentation fees	no.	33	\$ 300.00	\$ 9,900.00
Timber tree guards - supply + installation	no.	3	\$ 7,000.00	\$ 21,000.00
Picnic furniture and BBQ	no.	10	\$ 1,200.00	\$ 12,000.00
Bench seating	no.	27	\$ 1,500.00	\$ 40,500.00
Lamps including cable - supply + installation cost - \$5000.00	item			\$ 25,000.00
Playground equipment	item			\$ 20,000.00
Oval upgrade grass and irrigate (fully automatic system - supply + installation)	item			\$ 5,000.00
Median upgrade and grassing	item			\$ 2,800.00
Signs	item	4	\$ 700.00	\$ 2,800.00
Sub total plus 10% contingency	item	10%		\$420,000.00
STAGE FOUR				
TR EES - supply + plant	no.	10	\$ 500.00	\$ 5,000.00
Cotton Palms	no.	50	\$ 500.00	\$ 25,000.00
Plane Trees	m2	462	\$ 10.00	\$ 4,620.00
Street intersection and around memorials on Mostyn and Barker Streets,	item			\$ 1,000.00
MISCELLANEOUS				
Design Documentation fees	no.	4	\$ 2,000.00	\$ 8,000.00
Gas lamps including gas supply	item			\$ 10,000.00
Campbell and Barker Street roundabout sculpture	item			\$ 120,000.00
Western Oval Reserve pavilion and toilet - design + construction	item			\$ 120,000.00
Sub total with 10% contingency	item	10%		\$ 900,000.00
TOTAL				



### 5.03 Management and Maintenance

Management of the strategy is as important as the strategy itself.

Pride of Place funding and other funding sources for capital works and other projects will depend to a large degree on Council having the strategy in place and then following its intent, and its priorities.

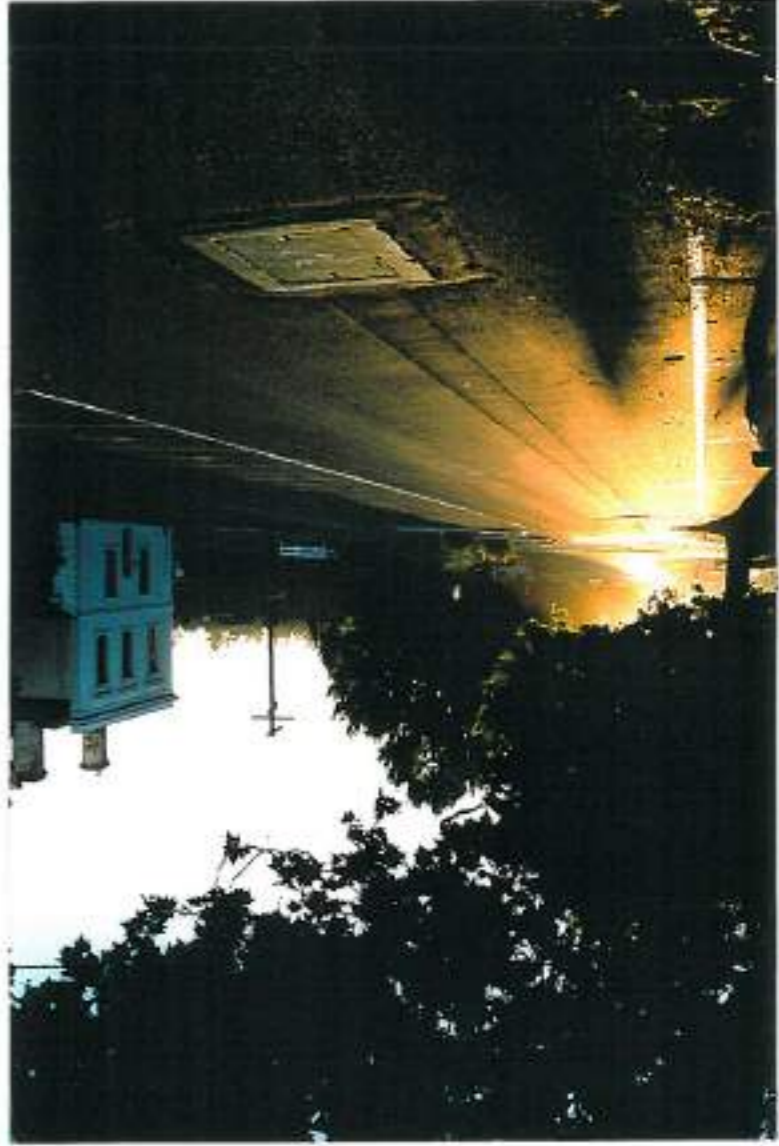
Decisions to be made by Council which can affect the physical character of the town or its commercial vitality need to refer to the both the Municipal Strategic Statement and the Urban Design Strategy and demonstrate the relevance to their recommendations.

At a more detailed level, it is important for the community to maintain an interest and an involvement in the implementation of the strategy. We recommend establishing a community advisory group titled the "Streetscape Advisory Committee" representing the arts, horticulture, heritage, tourism, general commerce and youth interests to assist Council with detail decisions affecting all aspects of the strategy implementation.

An important part of implementation will be maintenance, in particular, maintenance of trees and other planting. While it is understandable that Council staff may resent such intrusion, it is not possible for Council staff to take the degree of interest necessary as well as maintain all other street trees, parks and gardens throughout the Shire. The advisory committee could assist Council by being their eyes and ears and by coordinating volunteer organisations to assist with tasks that are beyond Council's resources. Council is a body that represents the community and does what it can to maintain the physical infrastructure and other community services. Council needs assistance and the sustained interest of the community if it is to provide what may in some cases be over and above their normal functions.



APPENDIX





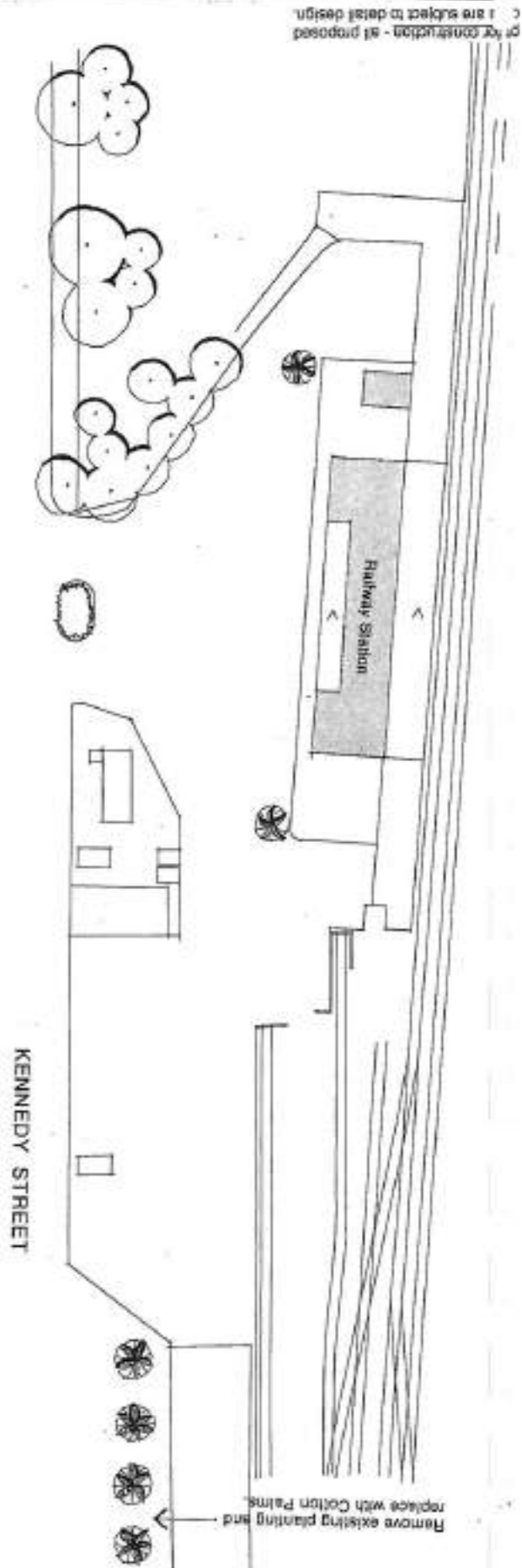
PLANS





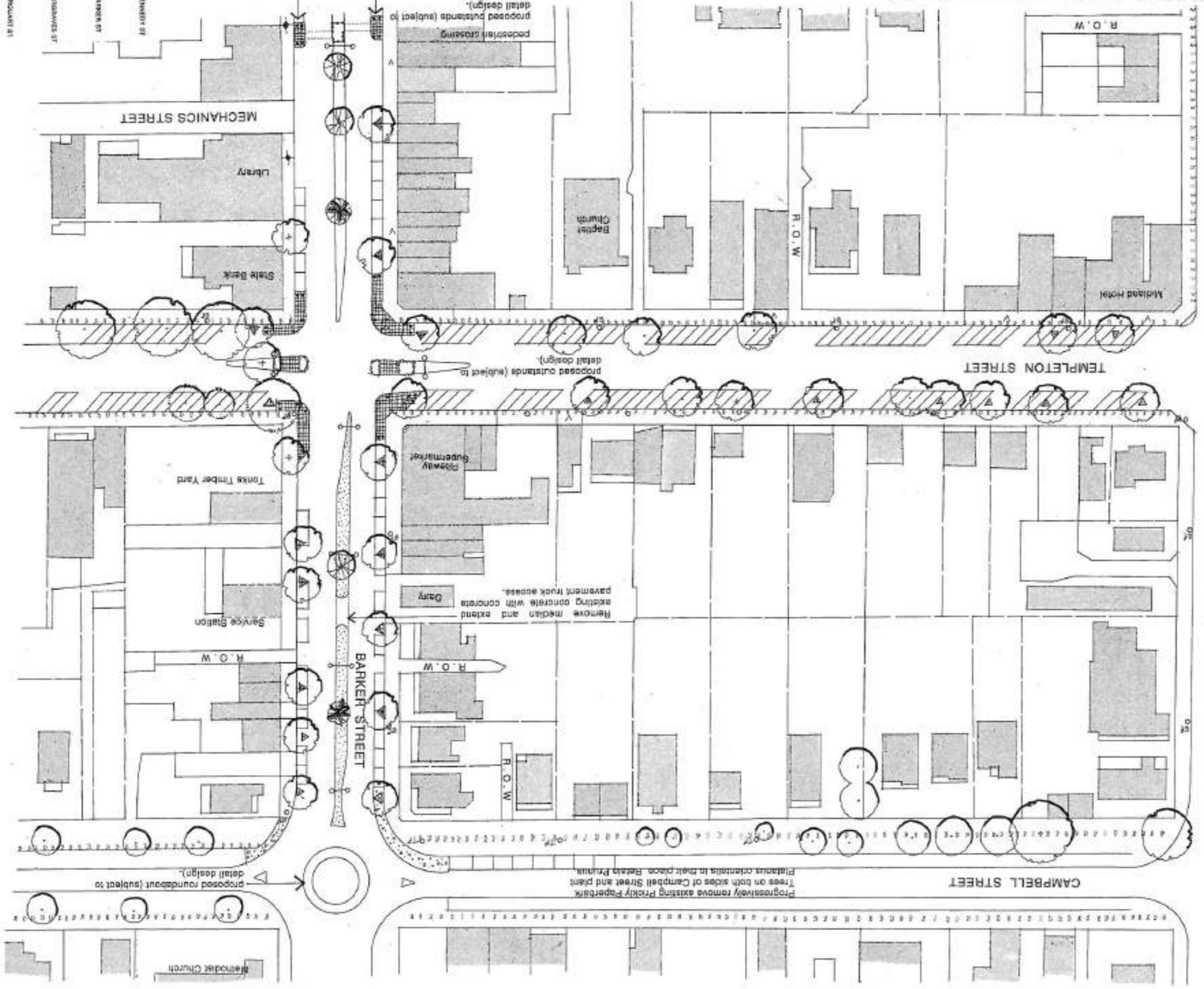
RT ALEXANDER SHIRE COUNCIL  
 CABTLMAINE URBAN DESIGN STRATEGY  
 MODEL  
 NO. PROPOSED TREE PLANTING / STREET FURNITURE /  
 OUTSTANDS AND CARPARKING  
 DATE May 1998

Geoff Sanderson  
 CONSULTANT  
 113 QUEEN STREET 2ND FLOOR  
 ADELPHI CHURCH  
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 WA 5006 AUSTRALIA  
 TEL (08) 8431 1144  
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of construction - at proposed  
 are subject to detail design.

Remove existing planting and  
 replace with Cotton Palms.



Progressively remove existing Prickly Paperbark  
 Trees on both sides of Campbell Street and plant  
 Platanus orientalis in their place. Retain Prunus

Remove median and extend  
 riding concrete with concrete  
 paving truck access.

proposed outstands (subject to  
 detail design).

proposed outstands (subject to  
 detail design).  
 pedestrian crossing  
 Cast Iron bollards to match  
 existing.

proposed roundabout (subject to  
 detail design).

CAMPBELL ST  
 TEMPLETON ST  
 MECHANICS ST  
 BARKER ST  
 KENNEDY ST  
 HARGREAVES ST  
 CROCKETT ST

MECHANICS STREET

TEMPLETON STREET

BARKER STREET

CAMPBELL STREET

KENNEDY STREET

Methodist Church

Service Station

Tankas Timber Yard

State Bank

Library

Baptist Church

Midland Hotel

Railway Station

R.O.W

R.O.W

R.O.W

R.O.W

R.O.W

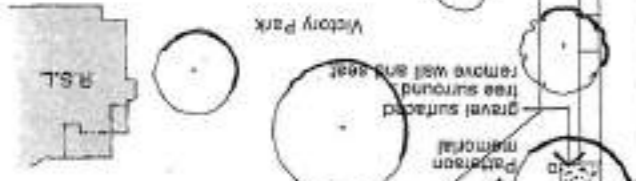
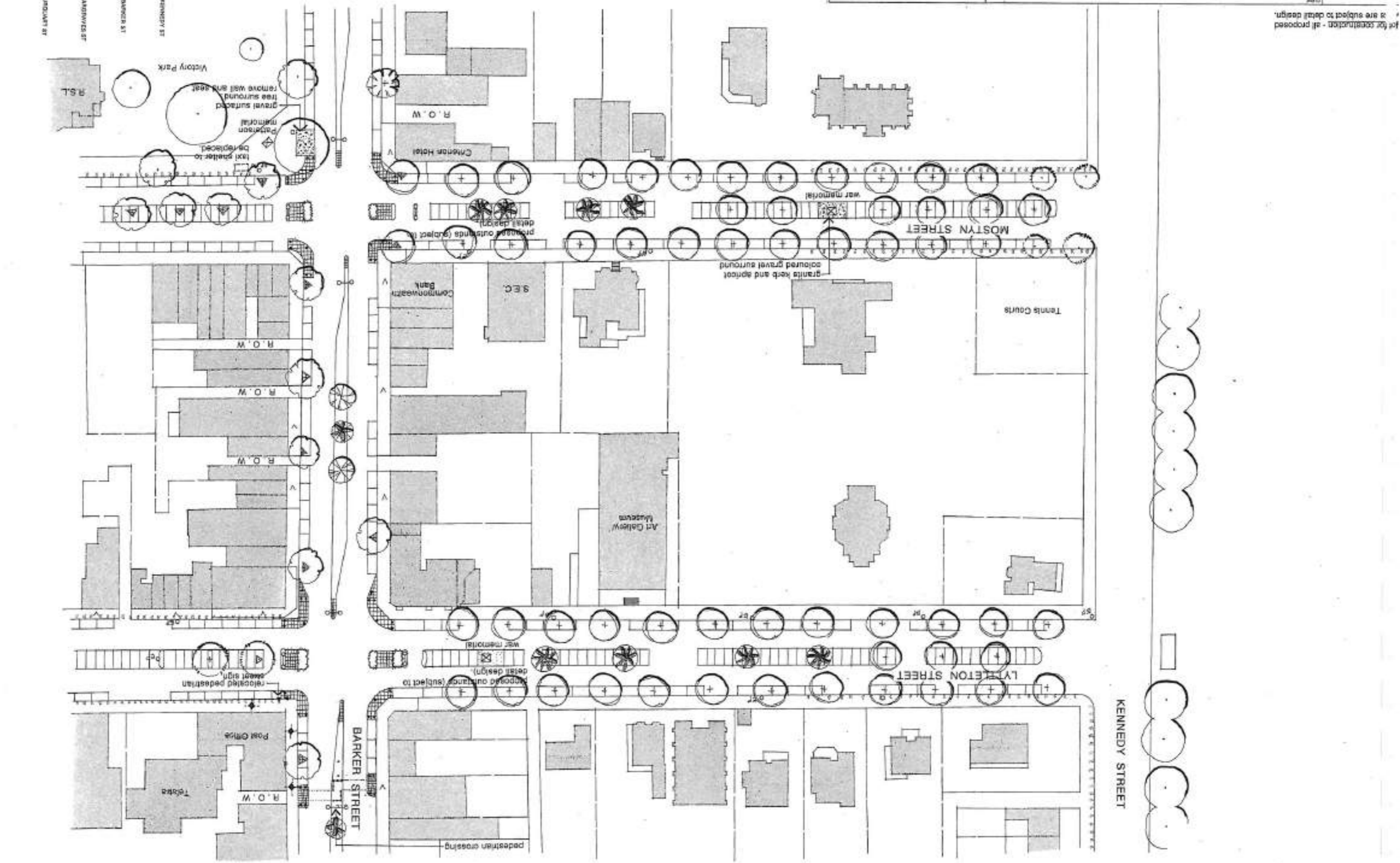


for construction - all proposed  
are subject to detail design.

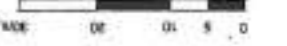


CLIENT: MT ALEXANDER SHIRE COUNCIL  
PROJECT: CASTLEMAINS URBAN DESIGN STRATEGY  
TITLE: PROPOSED TREE PLANTING / STREET FURNITURE / OUTSTANDS AND CARPARKING

DATE: May 1998  
DRAWN BY: [Redacted]  
CHECKED BY: [Redacted]  
PROJECT NO: 1000  
SHEET NO: 2

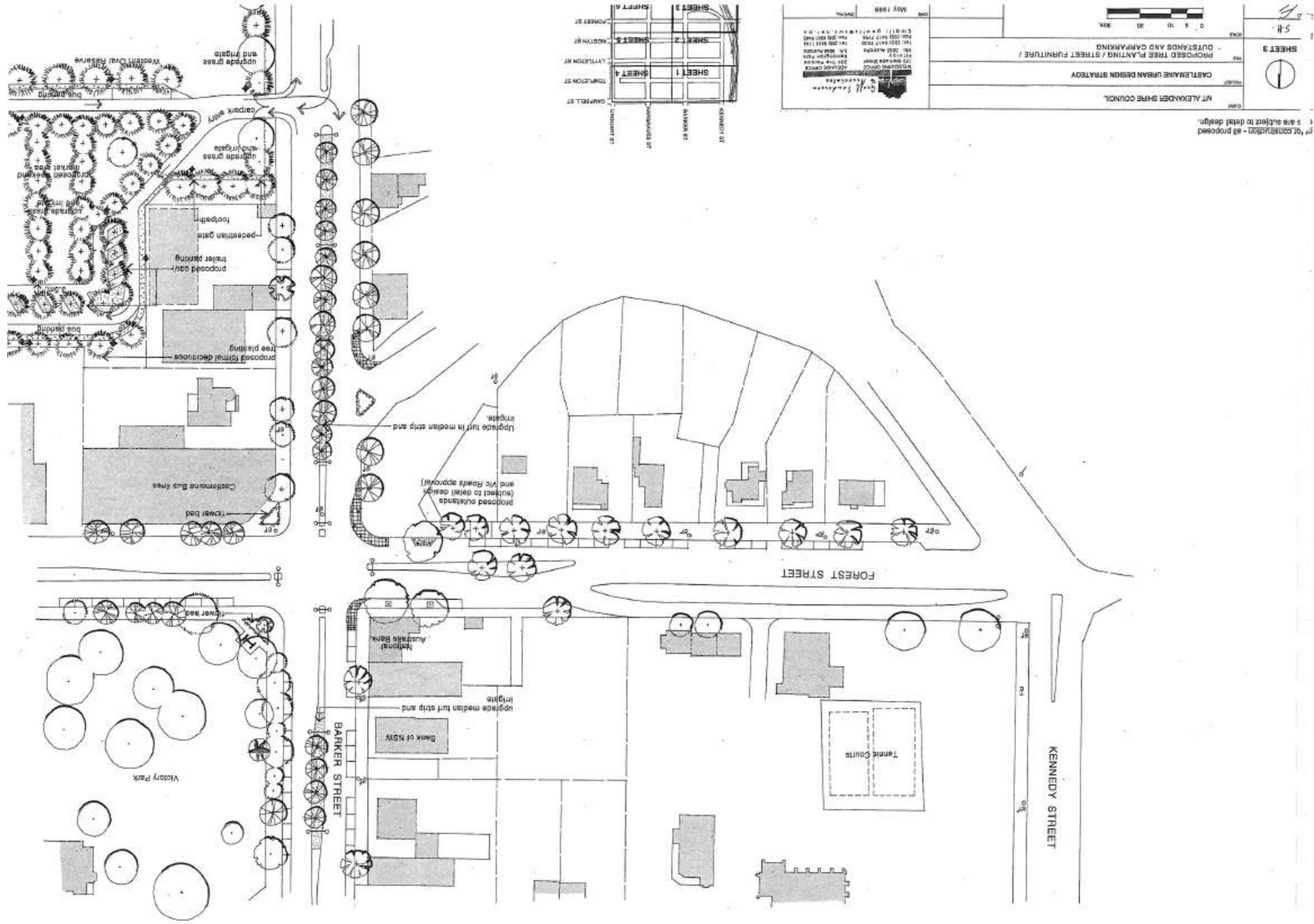






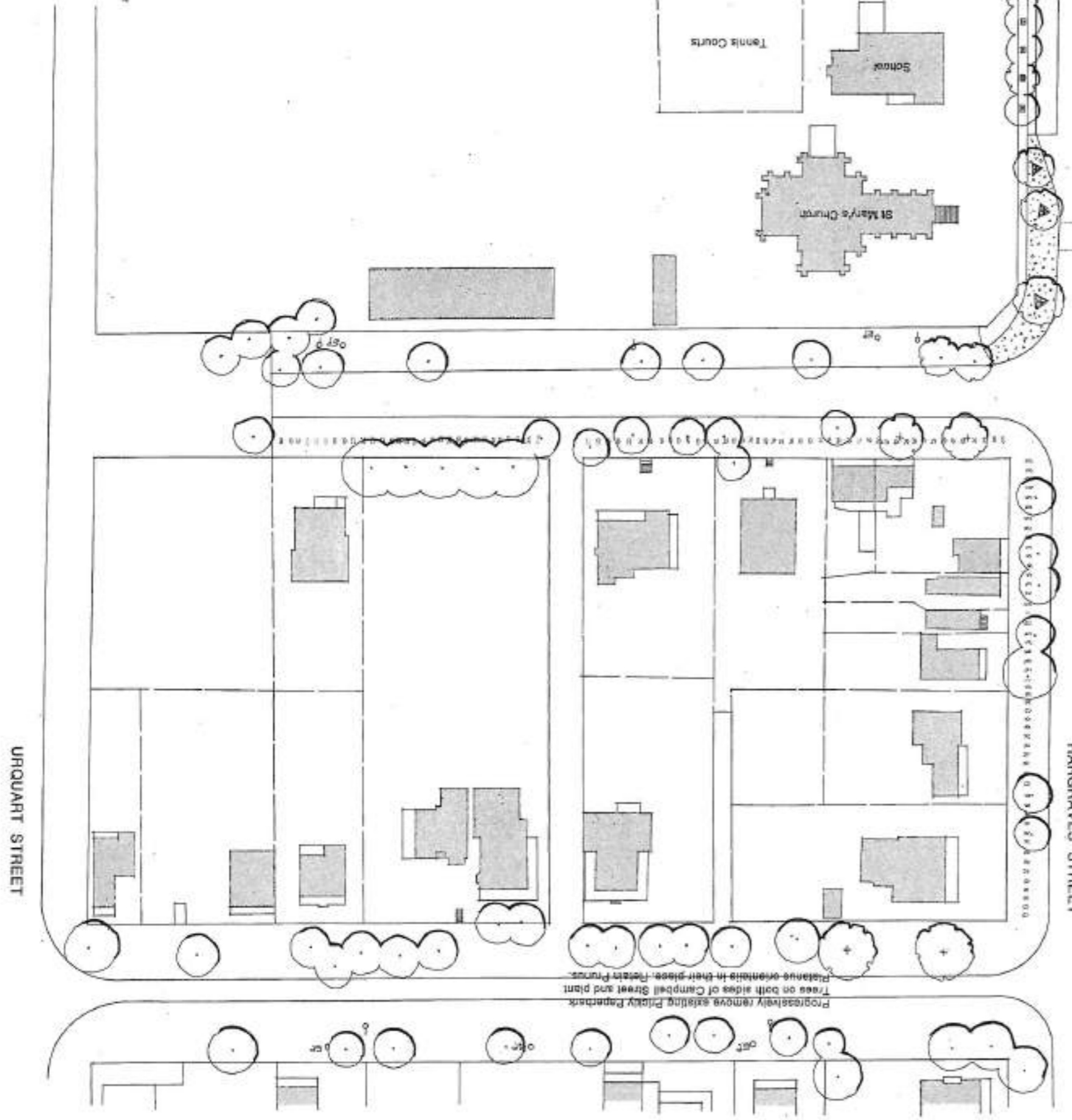
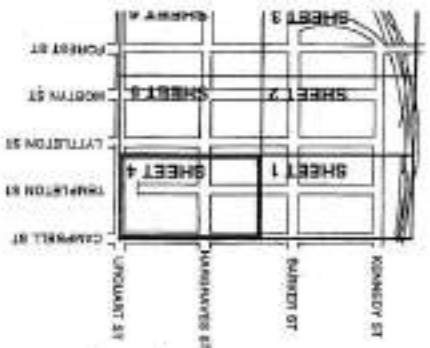
CLIENT: MT ALEXANDER SHIRE COUNCIL  
 PROJECT: CASTLEMAINE URBAN DESIGN STRATEGY  
 TITLE: PROPOSED TREE PLANTING / STREET FURNITURE / OUTSTANDS AND CAMPARKING  
 DATE: MAY 1998

DESIGNED BY: Gifford Saunders  
 CONSULTANTS: Gifford Saunders & Associates  
 175 BRUNNEN STREET, 2ND FLOOR, CASTLEMAINE, NSW 2142  
 PHONE: (02) 8817 2788 FAX: (02) 8817 1144  
 5/01/1 GIFFORD SAUNDERS & ASSOCIATES



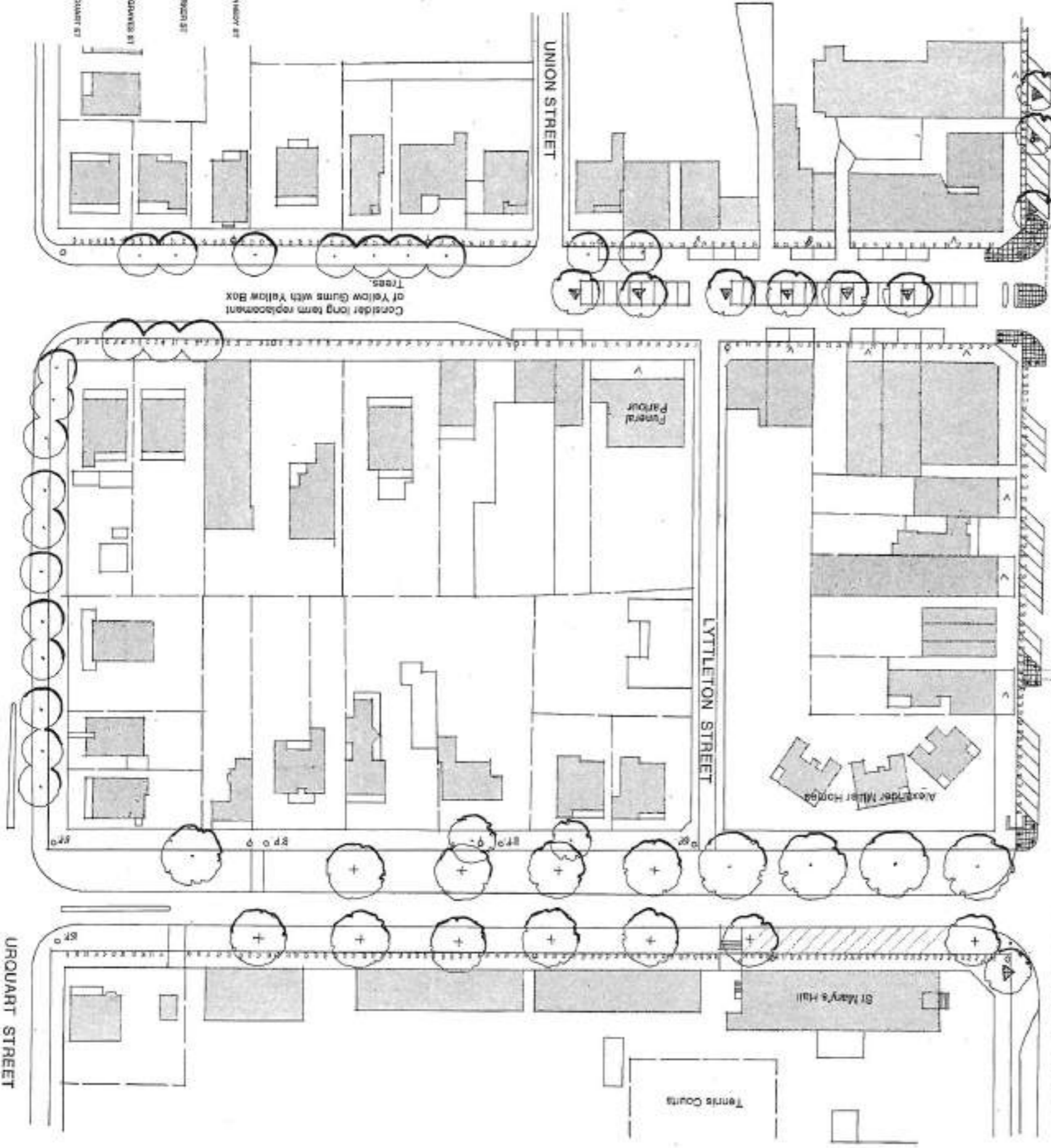
All proposed plantings are subject to detail design.







POWELL ST						
ROSTYN ST	SHEET 2	SHEET 3				
LYTTLETON ST	SHEET 1	SHEET 4				
TEARLETON ST						
CHAMBERLAIN ST						
CHANDLER ST						
WINDYBAY ST						
BARRETT ST						



DATE: MAY 1998

PROJECT: CASTLEMAINE URBAN DESIGN STRATEGY

TITLE: PROPOSED TREE PLANTING / STREET FURNITURE / OUTSTANDS AND CARPARKING

CLIENT: MT ALEXANDER SHIRE COUNCIL

SCALE: 1:1000

PROJECT NO: 1000

DESIGNER: Goff Sawchenko & Associates

100 GARDNER STREET, MOUNT ALEXANDER VIC 3087

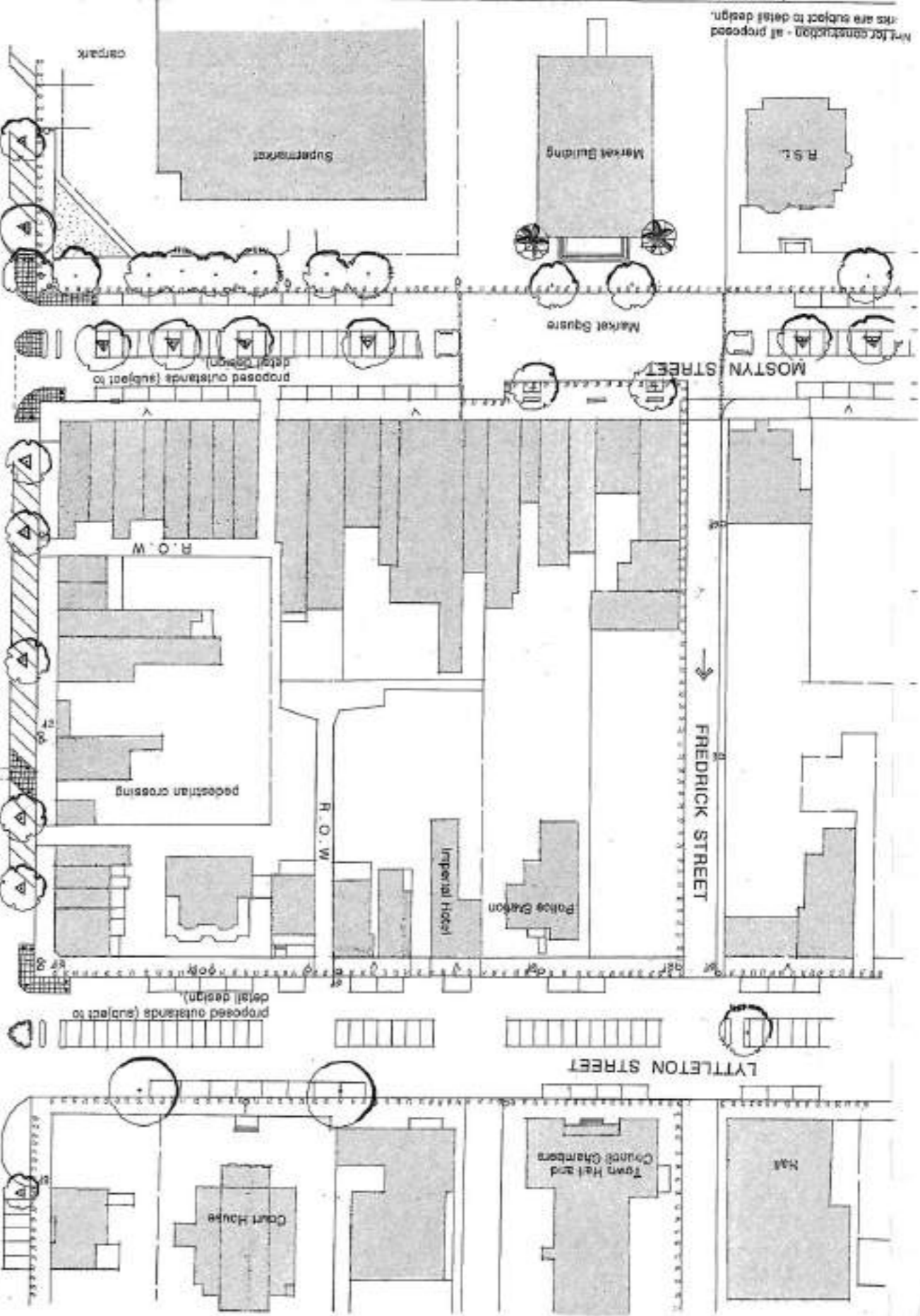
PH: (03) 9411 1111

FAX: (03) 9411 1112

EMAIL: GSA@GSA.COM.AU

PROJECT NO: 1000

DATE: MAY 1998









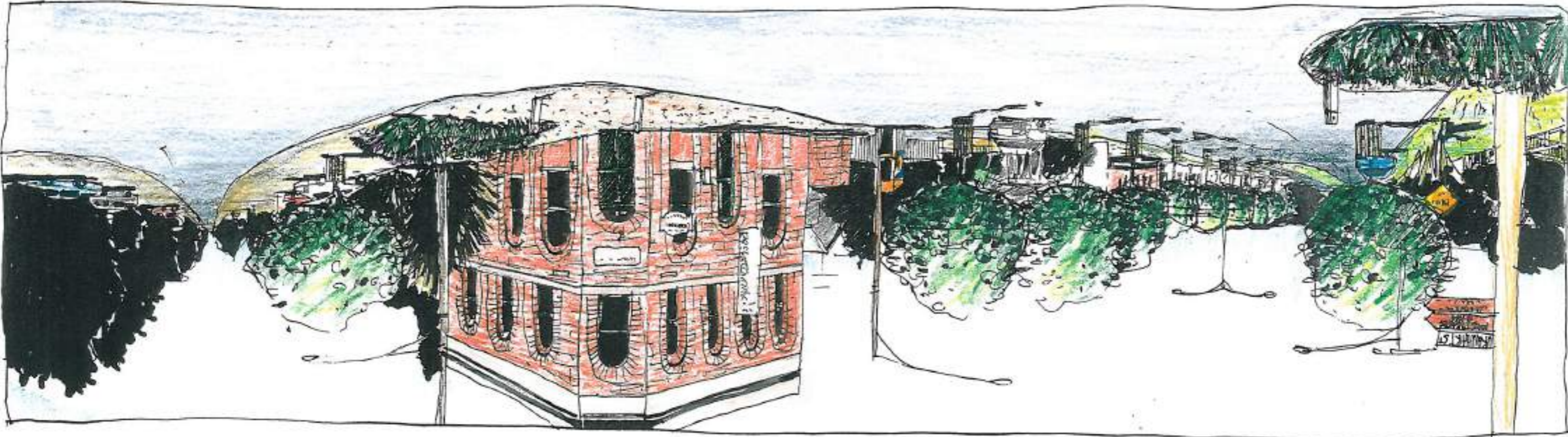
URBAN DESIGN DETAILS







Urquart Street we would prefer to remain as much as it is with the wide gravel verge. Where possible, Oriental Plane Trees should be planted and any tree replacement should be done with this species.  
 Forest Street needs to 'play down' its highway image and develop a town centre feel by planting where possible, at the junction of parking bays, with Oriental Plane Trees each with the traditional picket surround.



The outstand in the foreground would be surfaced with a gravel as per the entrance to the Botanic Garden. This colour complements the brick colour of the corner building and provides a generous 'apron'. Garden beds with Agapanthus, Iris or English Lavender would complete the picture.  
 Two simple timber bollards painted off white would protect pedestrians.

URQUART STREET/FOREST STREET



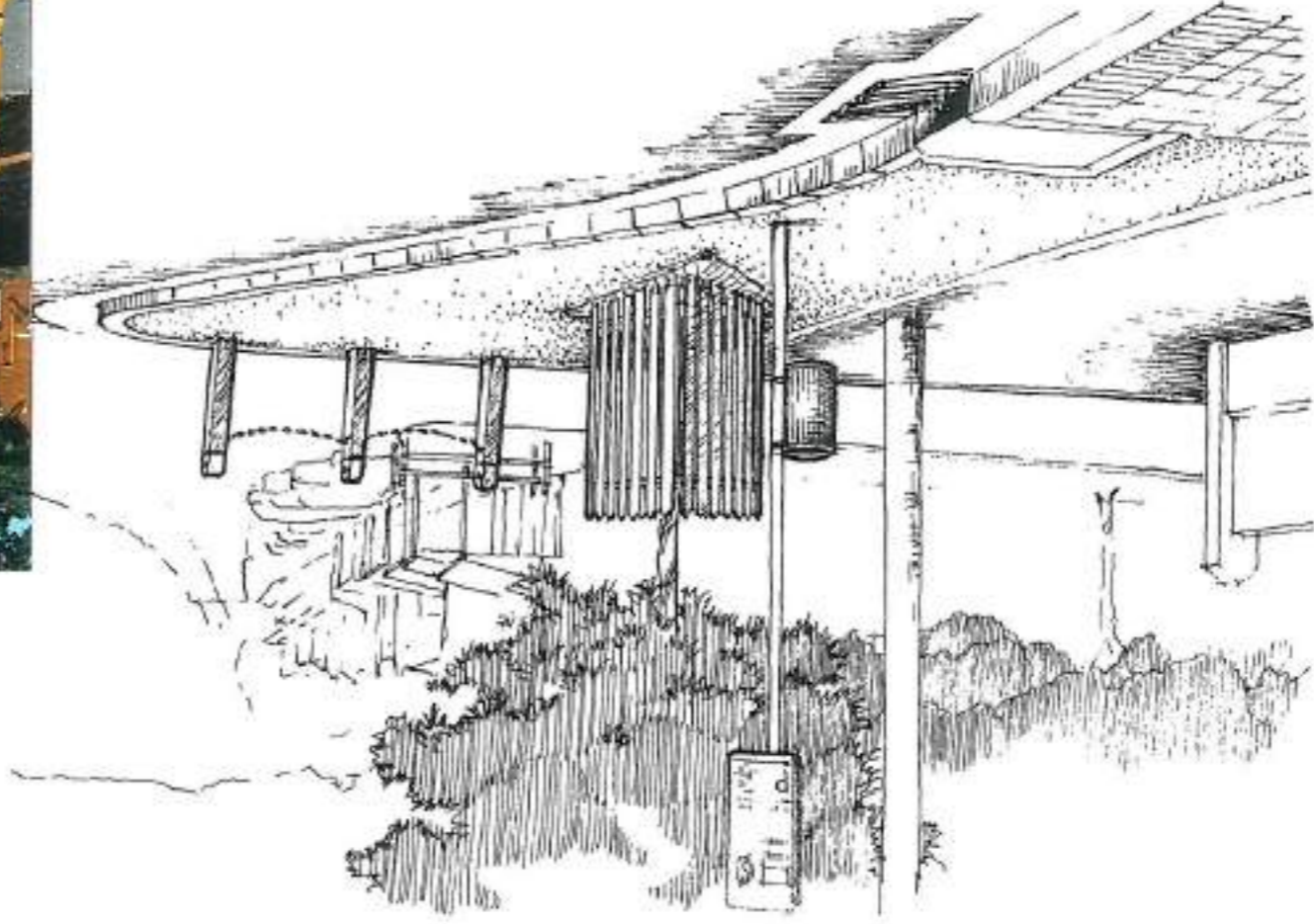


## LYTTELTON STREET EAST

We recommend the planting of Plane Trees all along the north side and to fill the gaps on the south side.

The roundabout deserves a massed planting of English Lavender - an old fashioned plant appropriate to Castlemaine.





The treatment of this corner was not appropriate and needs to be both simplified and made more relevant to the town's traditional use of materials and details.

The revised approach shows timber bollards and chains - a detail once used in the forecourt to the building which still stands (though much altered) on the north-west corner of Mostyn Street / Hargraves Street intersection. The surface would be gravel; the same as the Botanic Garden entrance. The stone kerbs and channel should remain.



HARGRAVES STREET /  
LYTTELTON STREET



# KENNEDY STREET

The west side of town is dominated by the Railway. Some of the elements which effect this dominance have been lost and should be reinstated.

The photo Top Centre is one of the most valued vistas from Kennedy Street a view which should be neither screened or compromised in any way.

Top Left shows the avenue of Yellow Gums and Sugar Gums which offer a 'tough' Australian edge to the formal European styled town centre.

Centre Left: another fine vista toward the Midland Hotel, a famous but dignified watering hole for weary train travellers Bottom left: the stone foundation of the 'Steam era' water tank, should be preserved and included on any descriptive tour of Castlemaine

Centre Bottom: illustrates the original style of railway steel post and rail barrier (left side)

On the right hand side is the ubiquitous, crudely crafted and inappropriate pine post and rail which should be removed and replaced with the traditional metal style of barrier.

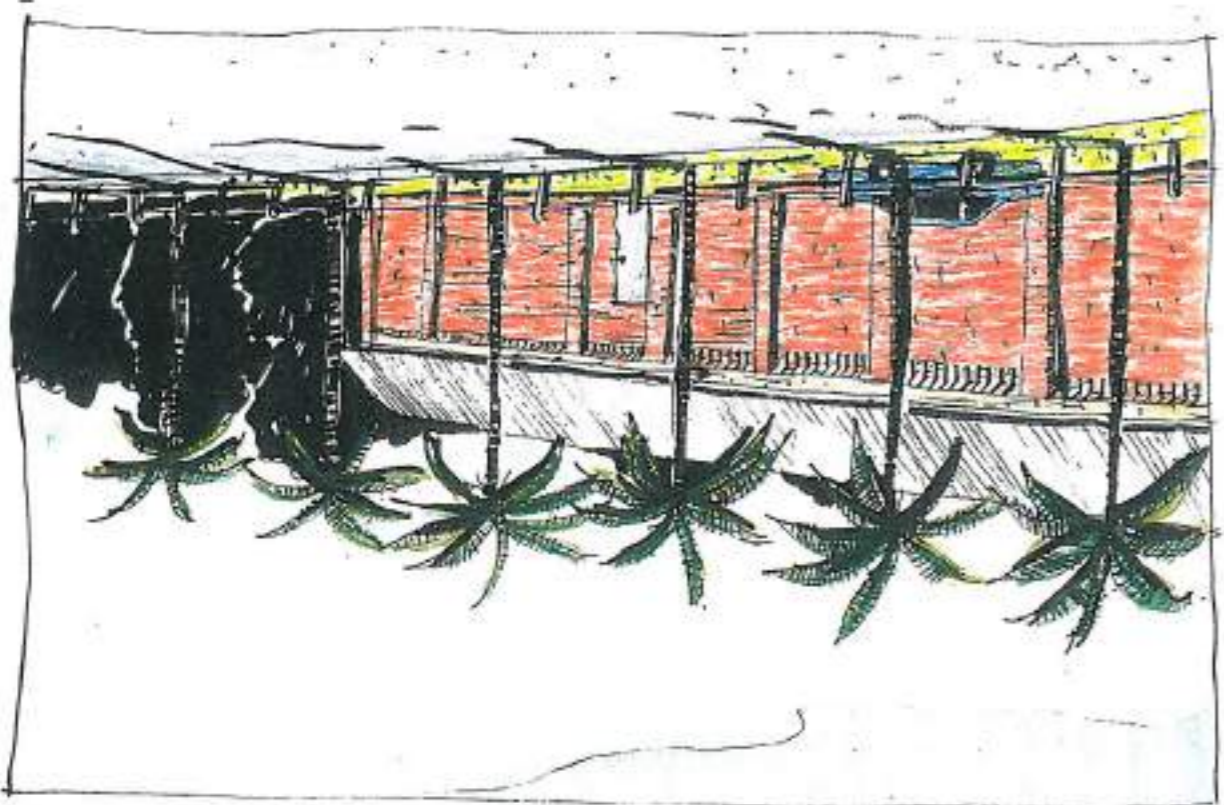
Top Right: is a barbed wire security fence which has no purpose and is unattractive. It should be removed and the metal barrier put in its place.

Bottom Right: the informal planting associated with the railway shed has no place in such a context, it obscures the building from view and should be removed. A formal row of Cotton Palms with a metal railway barrier to protect them, should replace the Eucalypts





CASTLEMAINE RAILWAY



We recommend removal of the shrubs and the V/LINE sign. The roundabout should then be planted with annuals and either side of the station building, a Cotton Palm planted. We also propose flood lighting the east facade. In this way the station will resume its prominence and importance. The shed also needs to be exposed rather than hidden and it too needs a row of Cotton Palms to grace it. We would like to see the station carpark inexpensively reshaped to accommodate more all day parking as well as tour bus parking.







The Coffee Bean, like most of the cafe's has outdoor tables but it is also easy to see in



This A-frame sign illustrates an idea to use a Castlemaine traders logo on a standard frame. Instead of the variety of proprietary advertising boards.



The Family restaurant is easy to see inside and its light, airy feel is very welcome



Tonks Bros offer a decorative and intriguing display appealing to locals and tourists alike.



The fruit and veg shop has a colourful, good smelling display on the street and also allows views into the shop.

SHOPFRONTS 2





Alley ways are often overlooked when considering the overall appeal of a town. The two on the left are interesting, but the one on the right is mundane and uninviting.

Climbing plants on the walls and fences(eg Campsis chinensis) and trees at the end of the vista would improve the one on the right. There are other situations like these which attract the curious eye. If they are not interesting or attractive they should be made so with little effort, such as the view on the right.

## ALLEYWAYS





Castlemaine has some very fine Oriental Plane Trees many of which are well over 100 years old and some are around 130 years.

There are also some Elms, Cotton Palms, Canary Island Palms, English Oaks, Lombardy Poplars and Silver Poplars of similar age. It is probable that the botanist and Director of the Melbourne Botanic Garden advised on trees selection and supplied Castlemaine with seeds and cuttings as early as the mid 1850's.

In the 1960's, Prunus and Prickly Paper Barks were fashionable and Castlemaine has its share too.

We recommend that the Oriental Plane Tree is further planted in the Town Centre streets, as well as adding to the existing Prunus planting, eg Lyttleton Street east end and central median between Hargraves Street and Barkers Street, Barkers Street, and that they replace the Prickly Paper Barks in Campbell Street.

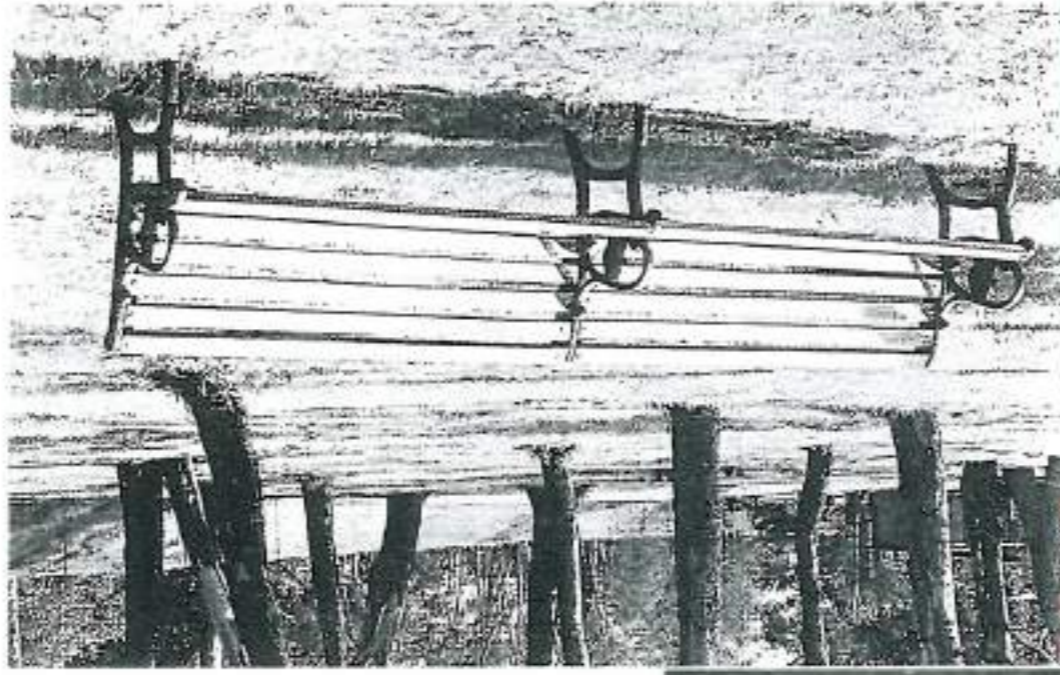
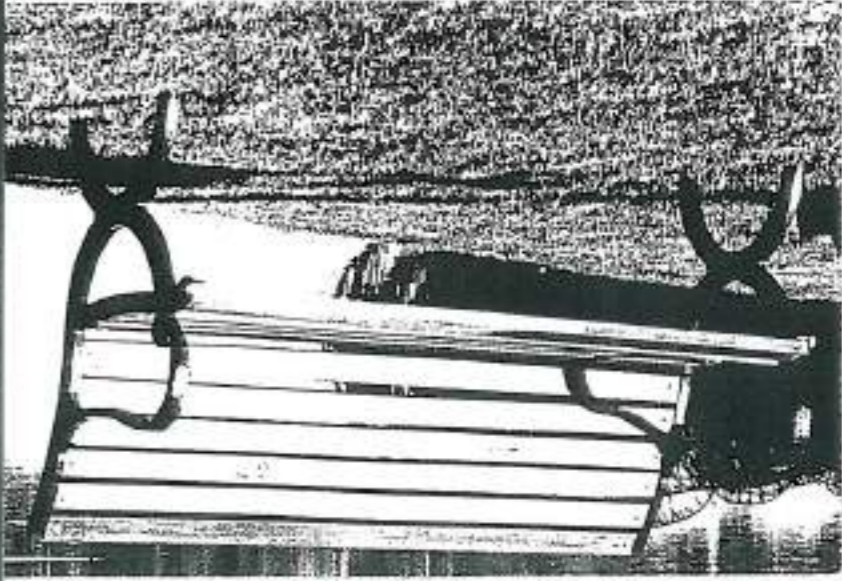
We also recommend some additional planting of Cotton Palms in Barker Street median and White Cedar Trees in Forest Street west end.

In Mostyn Street west end and Lyttleton Street west end we recommend Hackberry Trees to provide shade for parked cars. The Canary Island Palms would remain dominant in the centre of both streets at the west end.

Along Forest Street between Barker Street and Urquhart Streets we recommend planting Oriental Plane trees where opportunity permits (see plans).

## TREES





**Litter Bins** The bin illustrated above right is currently used in Castlemaine and is quite suitable. The colour should be the same as cast iron furniture.



**SEATS & LITTER BINS**

**Seats** Early photographs show a seat similar to the one in the photo top centre. The original seat had the cast iron ends as per the photo but there was a centre leg similar to the photo bottom right and the seat was longer. We recommend using the original design and altering the existing seats or use the one illustrated below right which is available from Shepparton. The other two seats illustrated would also be suitable. The iron frames should conform with other cast iron furniture and be painted red/brown to match the paint colour on the Old Telegraph building window frames.





The gallery has two bracket lamps which should also be turned on to display the attractive building facade



This verandah has lights and they need to be left on all night

# LIGHTING

In general, there is little need for additional street lighting except in Templeton Street between Barker Street and Hargraves Street on the north side. Western Reserve also requires lighting of footpaths and the proposed carpark.

Where additional lighting is needed in along commercial streets under verandahs. Such lighting is best provided by traders leaving on window display lighting and by installing bracket lamps on buildings (according to heritage advice or fitting down lamps to the under side of verandahs). There are several buildings in the town with external lights which should be left on, at least until midnight. The Gallery/Museum is one example. Several other grand buildings and monuments (eg Burke and Wills and old and new public sculptures) should be lit so that



A dark verandah space which could benefit from under verandah lighting.



Castlemaine Town Centre Urban Design Strategy

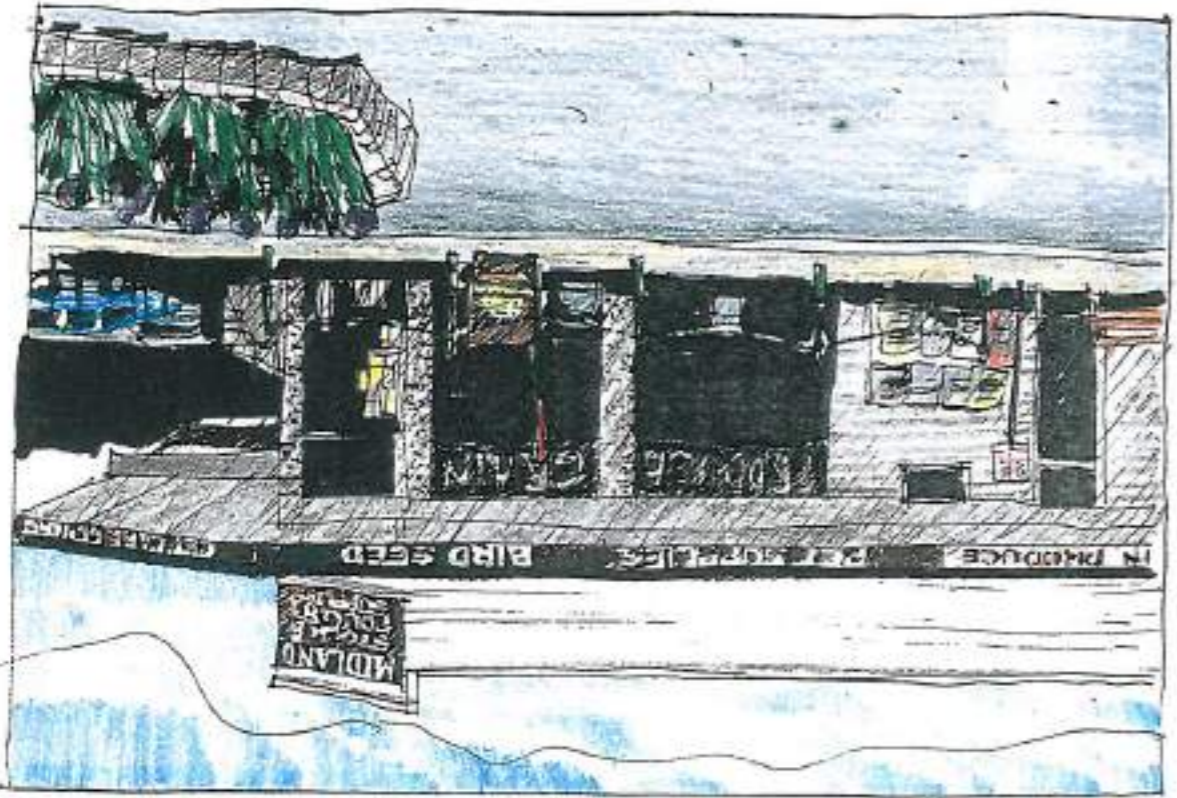
An original bracket lamp which should be left on at night



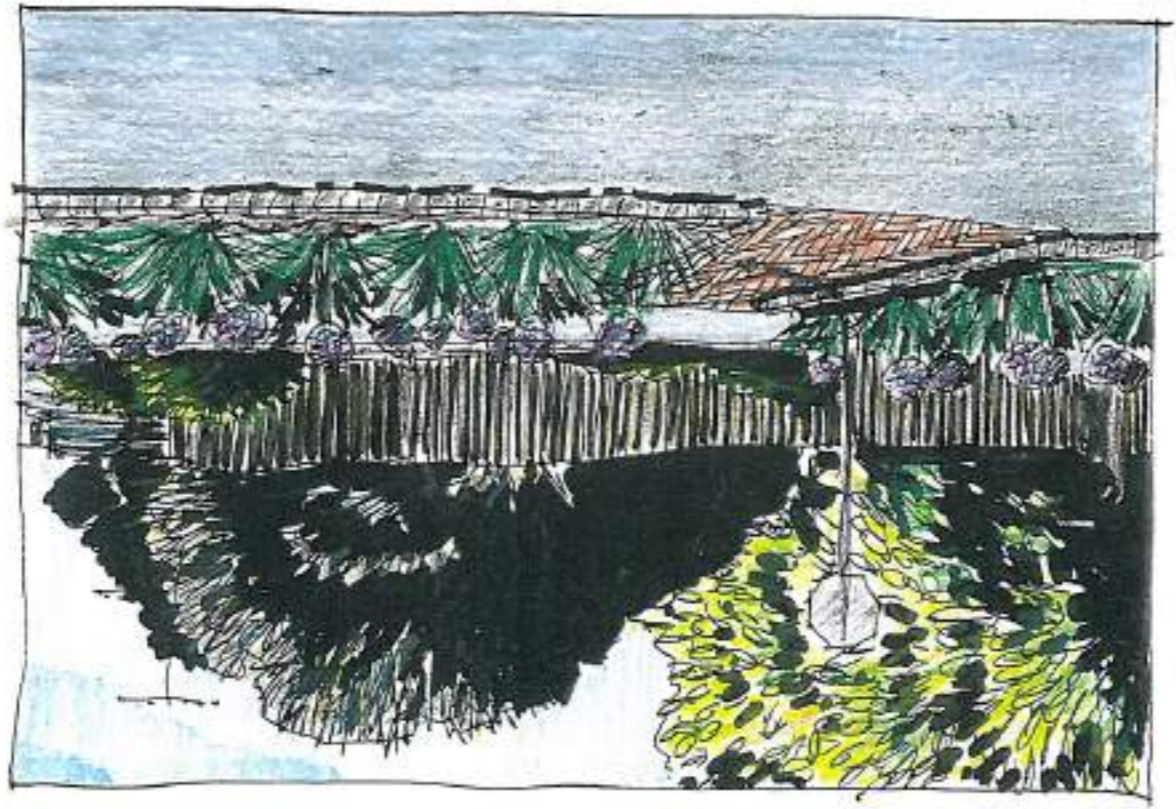
Barker Street is well endowed with street lighting but there are several fine buildings, the Patterson monument and under verandah spaces



**TRAFFIC ISLANDS**



The sketches illustrate preferred treatment of the islands i.e. Granite kerbs with a red brick on edge herringbone patterned pathway. Planting is Agapanthus, but could be English Lavender, bedding Geraniums or bedding Roses - they need to be plants which belong in a heritage setting. Stone kerbs are appropriate within the civic precinct but elsewhere they could be of precast concrete block or insitu concrete, in both instances finished similar to granite by sand blasting.



Castlemaine Town Centre Urban Design Strategy



**SHOPFRONTS**

A framed print of early produce advertisements and a story about the groceries typically sold in the 1850's to hungry miners would be of interest to the tourist. Instead words to advise people what to buy sell, use painted murals. Fruit and vegetables, meats, eggs and cheese to entice the shoppers and offer an attractive alternative to the new supermarket. Another window of Castlemaine smallgoods in model form would also be a tourist attractor. The pharmacy, like the newsagent offers nothing that distinguishes from most other pharmacies in the country. The window displays a variety of products people expect to buy rather than anything special. Anything which helps to reinforce the goldfields history of the town, the history of alchemy or a story about the first pharmacy in Castlemaine or early forms of goldfields surgery would do much to draw in the tourist especially if there were old photos and other memorabilia exhibit.

Bottles of Snake Oil and other all purpose, but harmless, remedies could be sold along with the regular pharmacy products.



The Rite-way supermarket is of traditional presentation however it too would benefit from a window display aimed at the tourist.



The window display lacks any sense of Castlemaine's history or any inviting display. The Lolli shop in Maldon is an example of the display image that is much more appropriate.



There are many good examples of shop window displays and signs. Some examples that could be improved without big expense or inspire other traders to do more to create a stronger image of Castlemaine's gold history and to entice tourists to stop and shop.

This is typical of newsagents across the country with posters covering the windows preventing views in and losing the opportunity to use the window space to appeal to tourists. The newsagent has an established local trade but it could expand that trade to include tourists if there was an attractive representation of Castlemaine's early newspapers and the opportunity to purchase copies in the shop.



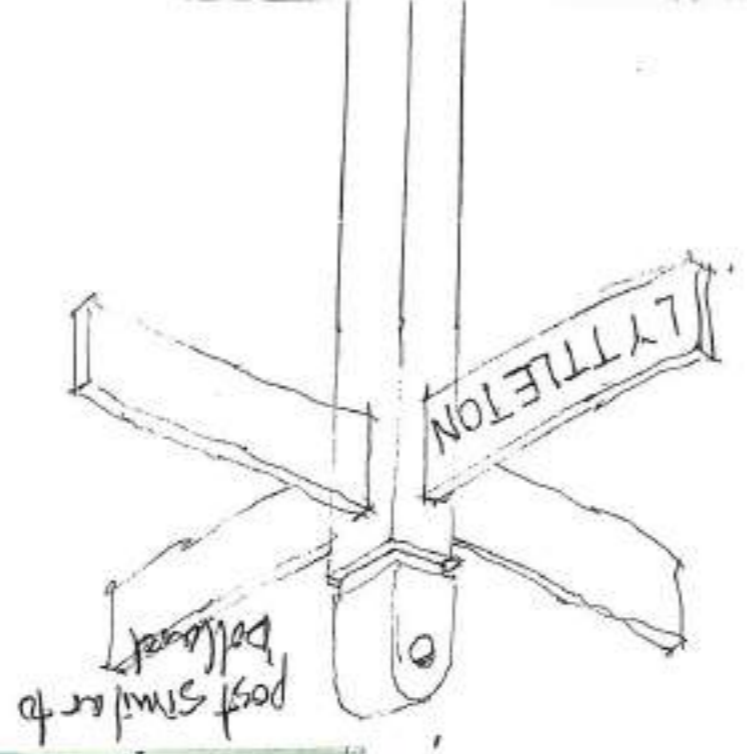
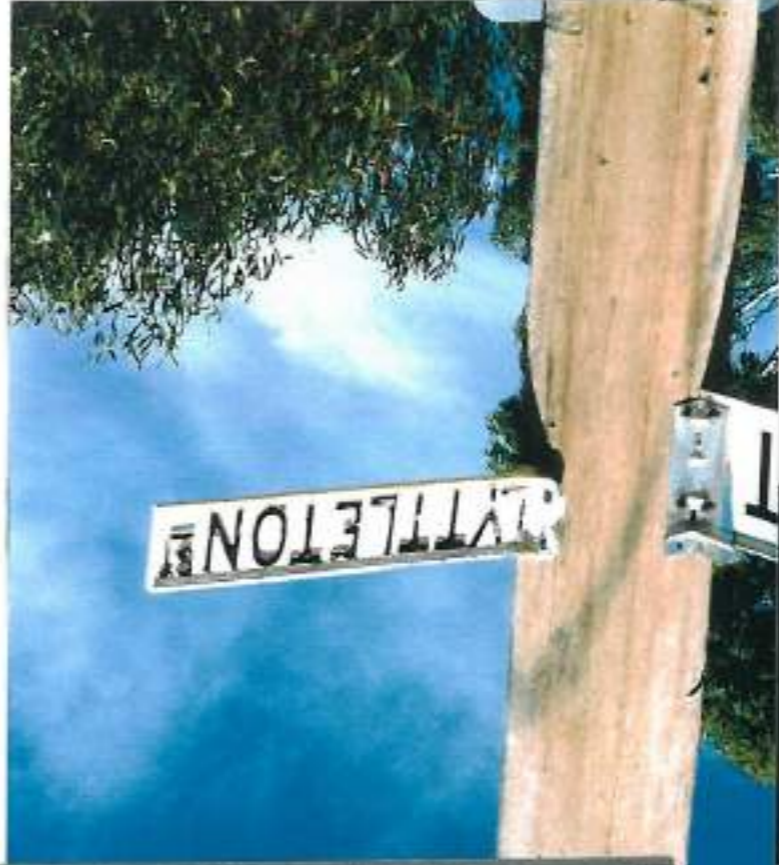
These A frames compete with one another and are unnecessary if the shops are sufficiently inviting. A frames should only be used in situations like top left where the Tea Rooms may not otherwise be seen, or as chalk boards to advise of daily changes in specials. Such boards should be of standard format with a Castlemaine Trader logo (see sketch).



This entry has a problem with glass reflection which prevents passers by from seeing in.







The cast metal sign, bottom centre, is typical of the Castlemaine street signs in the 19<sup>th</sup> Century. They were functional, lasted a long time and were easy to read.

We recommend retaining those which remain and using them for all future pedestrian directional signs (as per sketch).

The green ornamental sign illustrated in the other two photos should be retained but relocated to the footpath at the corner of Barker St and Lytleton St where it is easier for pedestrians to read. Such signs are not suitable for directing motorists.

Pedestrian direction signs should be limited to public places and services.

Commercial direction signs in streets, such as in Daylesford, are not recommended.

The photos top right and bottom right show how confusing signs can be when they collect together. Often symbols are more easily understood than words or symbols combined with words.

The Art Gallery, Senior Citizens, Railway Station, Churches, Police, Library can all be better presented as a simple image with lettering ( see sketch) using the international sign symbols which also help tourists from outside Australia

**SIGNS**



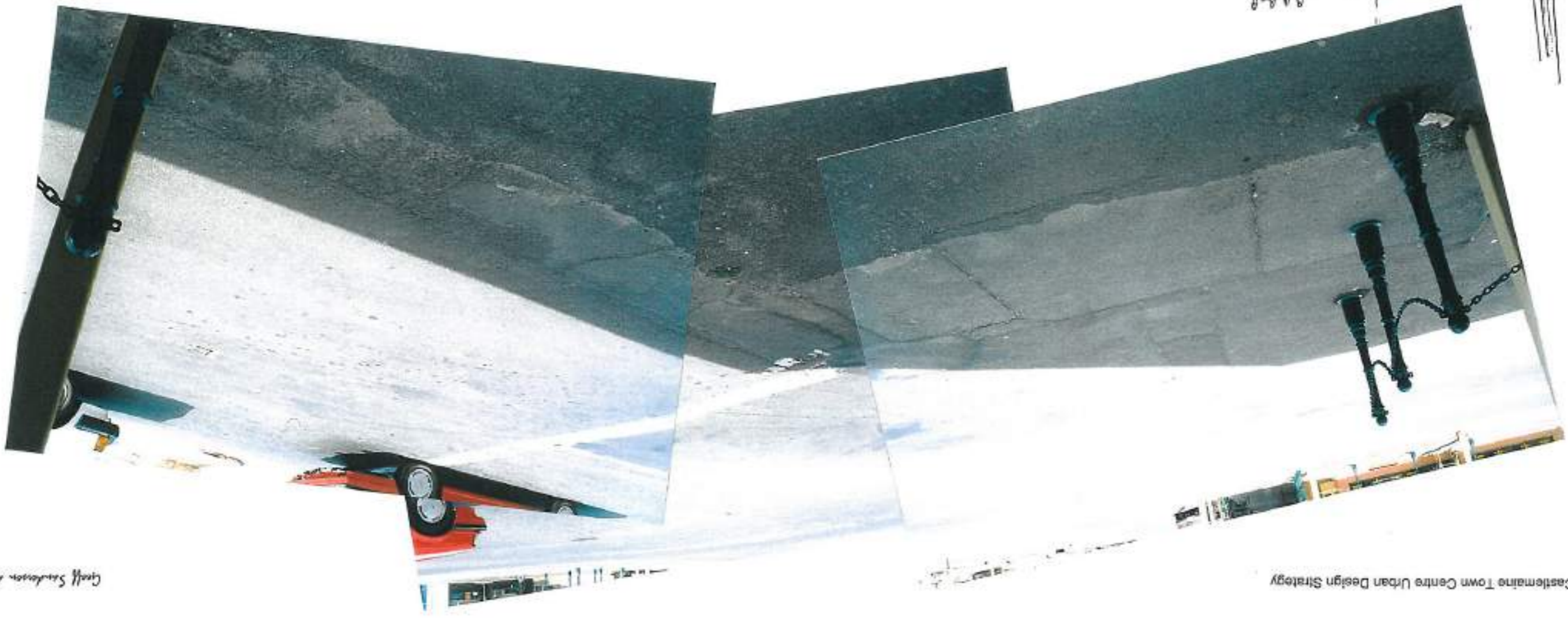
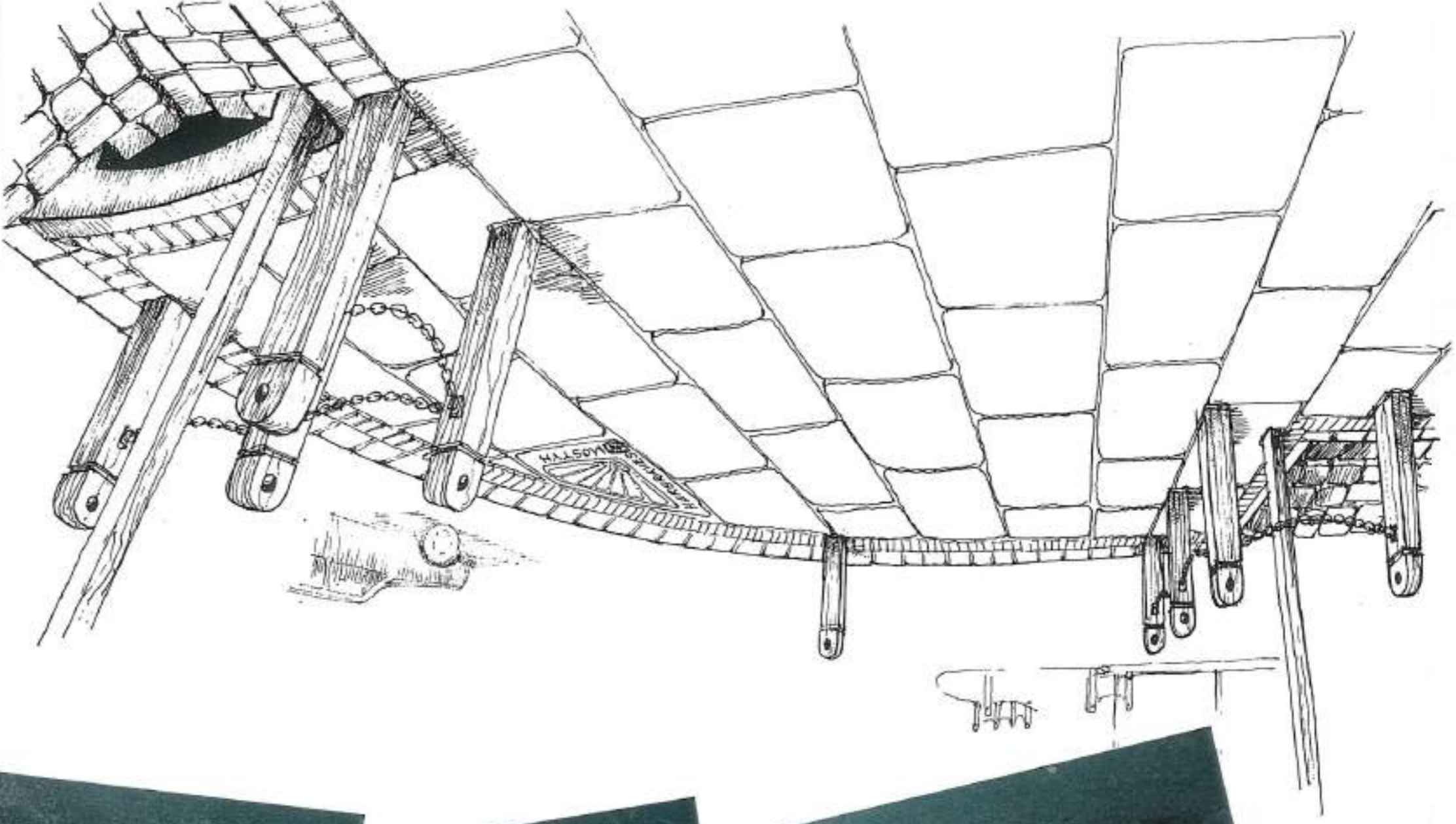
**TYPICAL OUTSTAND**

The intersections in the Town Centre are all difficult for pedestrians to cross given the dominance of the car environment.

The outstand detail illustrated projects into the road space for the width of a parking lane. It does not reduce the trafficable space. The outstands would have granite (or concrete precast, looking like granite) kerbs with red clay brick on edge as a trim. The larger module is 1m x 0.5m precast concrete in a dark slate grey.

Bollards are typical of those commonly used in Castlemaine. They would be made of sawn hardwood, shaped as illustrated and painted white lime colour. There is also an opportunity to inlay a feature as per the sketch top right. The inlay could have brass letters, mosaic tiles or a number of other variations; cost driven.

With the new outstands, the existing cast iron bollards could be relocated, repainted and used in the Civic precinct where they are appropriate.



Grant Sutherland and Associates

Castlemaine Town Centre Urban Design Strategy



**STREET DETAILS**



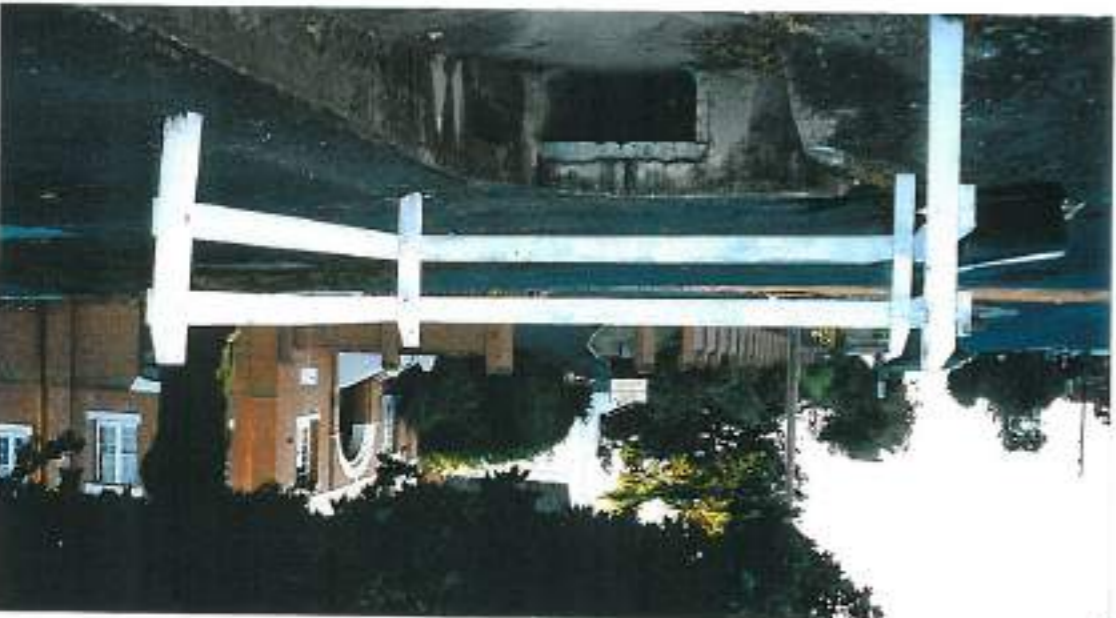
*Post and rail barriers around drain outlets and inlets should be as per those illustrated. (photos right above and left above)*

*We recommend continued use of this pavement pattern in dark grey coloured precast concrete. It is disappointing that the service installers did not properly reinstat the pavement, kerbs, including kurb tops and channels (photos below) need to be cleaned of bitumen and returned to their former glory.*

*Top left is a precast concrete pavement similar in pattern and colour to the original slate - little of which remains (Lyttelton St outside the Post Office) and it is both too expensive and hazardous (slippery) to consider.*

*All such details should be conserved and new works should not depart from them.*

*These photos illustrate some of the pavement, kerbs, channels and barriers which were commonly used in Castlemaine and form part of the town's character.*



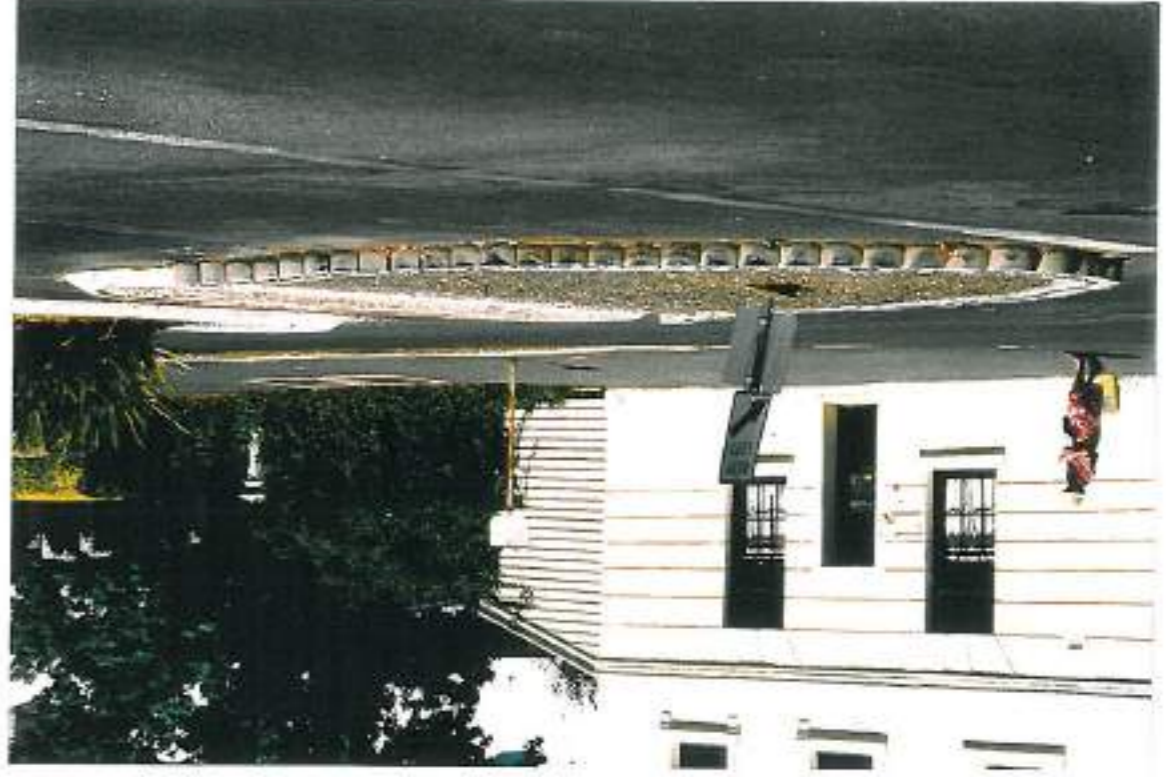


SOME MORE DETAILS

*Tree surrounds should only be in the timber picket detail shown at left. All metal surrounds as per bottom photo, should be replaced with the timber. Where timber picket surrounds could be damaged by vehicles, wheel stops should be installed. The gravel at the entrance to the Botanical Garden is the recommended material for all verges and other places where gravel is recommended.*







The sketches show additional tree planting within the centre median and the 'humanising' treatment of traffic islands. Precast concrete kerb to be replaced with granite kerbs.

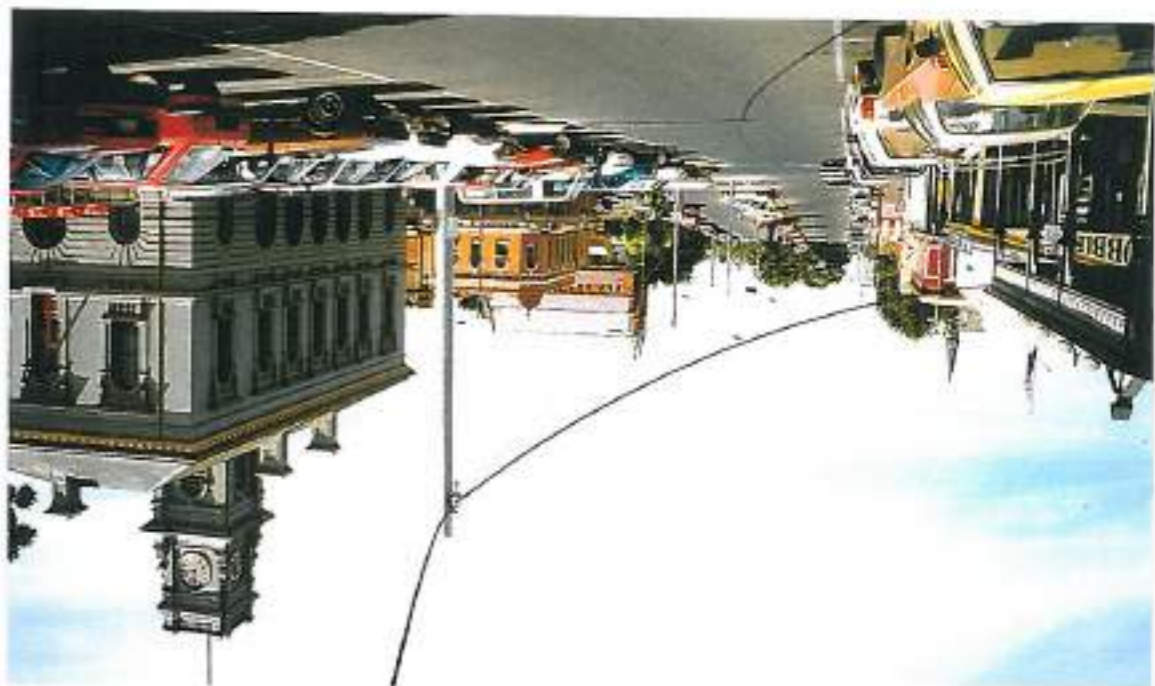
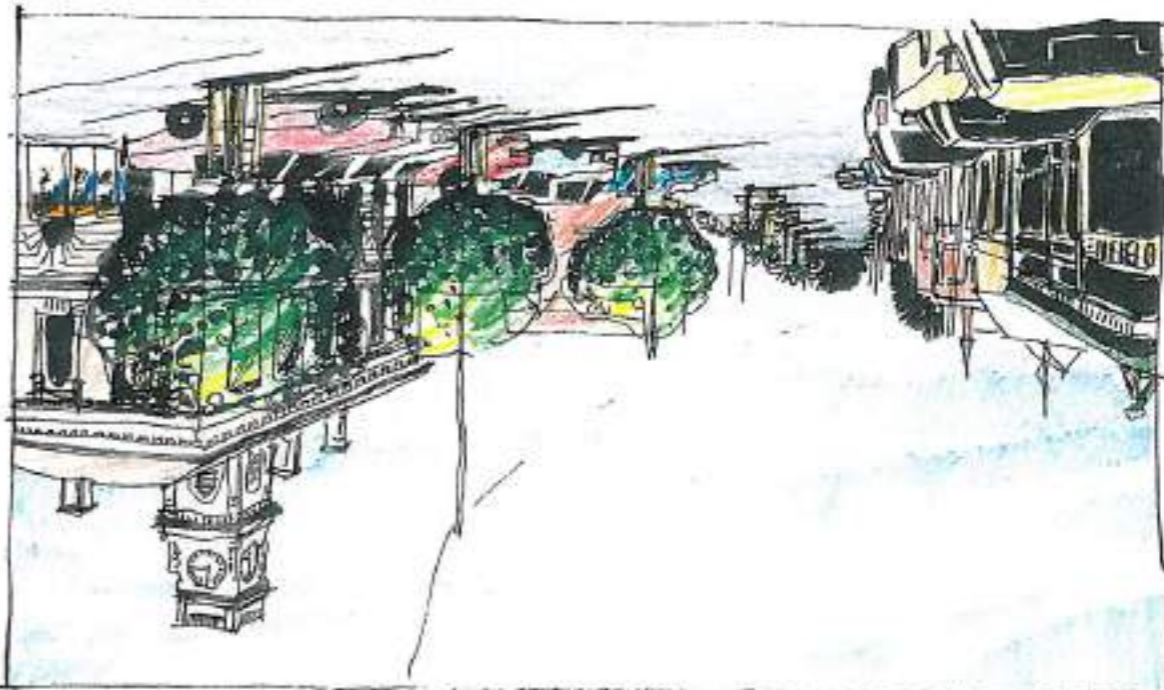




LYTTELTON STREET/  
CIVIC PRECINCT



The sketches show additional Oriental Plane tree planting within the centre median and the 'humanising' treatment of traffic islands - planted with Flag Irs or Agapanthus.







*We are recommending that it be expanded to include Frederick Street but reduce the use of Frederick Street as a thoroughfare.*

**FREDERICK STREET CARPARK**



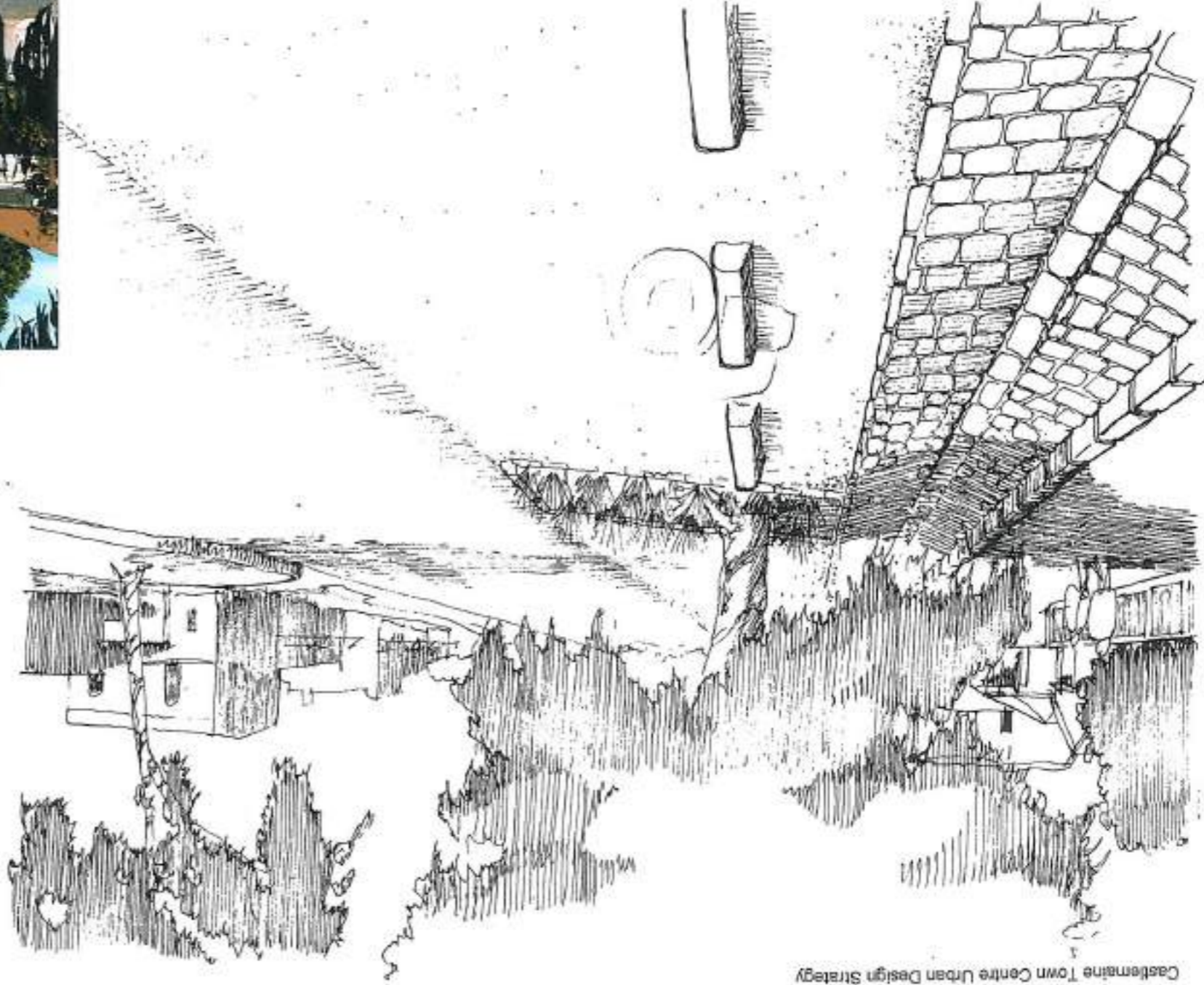


MOSTYN STREET EAST



Angle parking in this section of Mostyn Street could be provided as sketched. Wheel stops of precast concrete, finished to be similar to granite, would prevent cars rolling into the stone channel.

Kerbs and channels need to be regularly cleaned and maintained as an important feature of the streetscape. The carpark surface could remain as gravel, allowing the wheel stops to distinguish parking bays.



Castlemaine Town Centre Urban Design Strategy



The Market Square precinct study has made numerous recommendations including the creation of a square in front of the Market building. This recommendation we support.

Centre median parking is a fact of life in Mostyn Street and will remain. Accordingly we recommend planting Oriental Plane trees in approximately every fifth parking bay to shade parked cars and screen them from viewing points at the east and west ends of Mostyn Street.

The sentinel Cotton Palms either side of the Market Building are an essential part of its Indian Colonial imagery. It would be wrong to dilute this image by planting more Cotton Palms in Victory Park - despite the fact that they were once there.

We do not recommend planting trees on the north side of Market Square, as the verandahs provide adequate shelter and there is already some loss of parking due to the square.

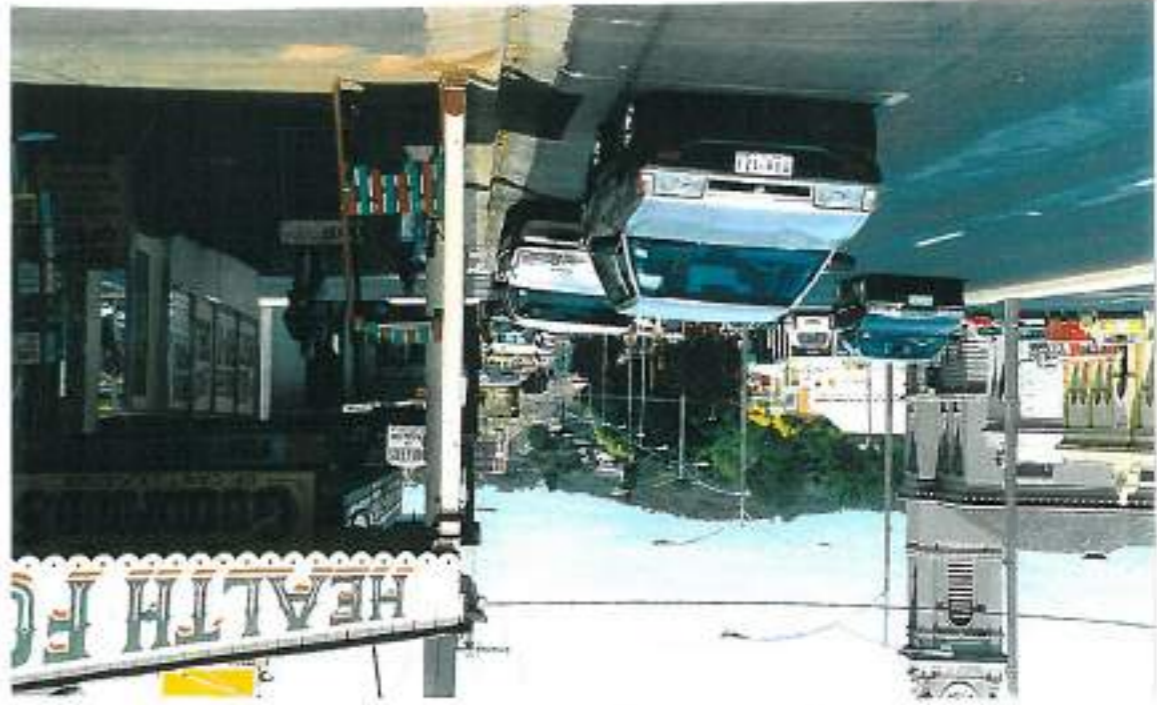
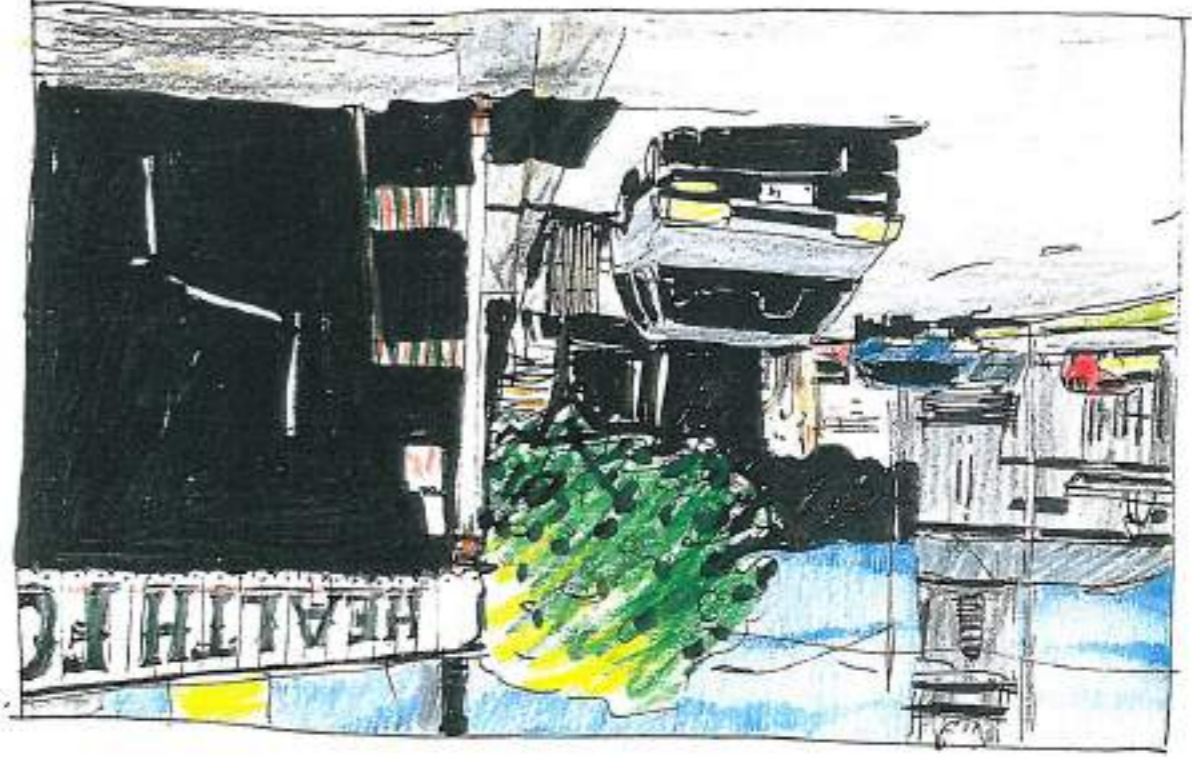
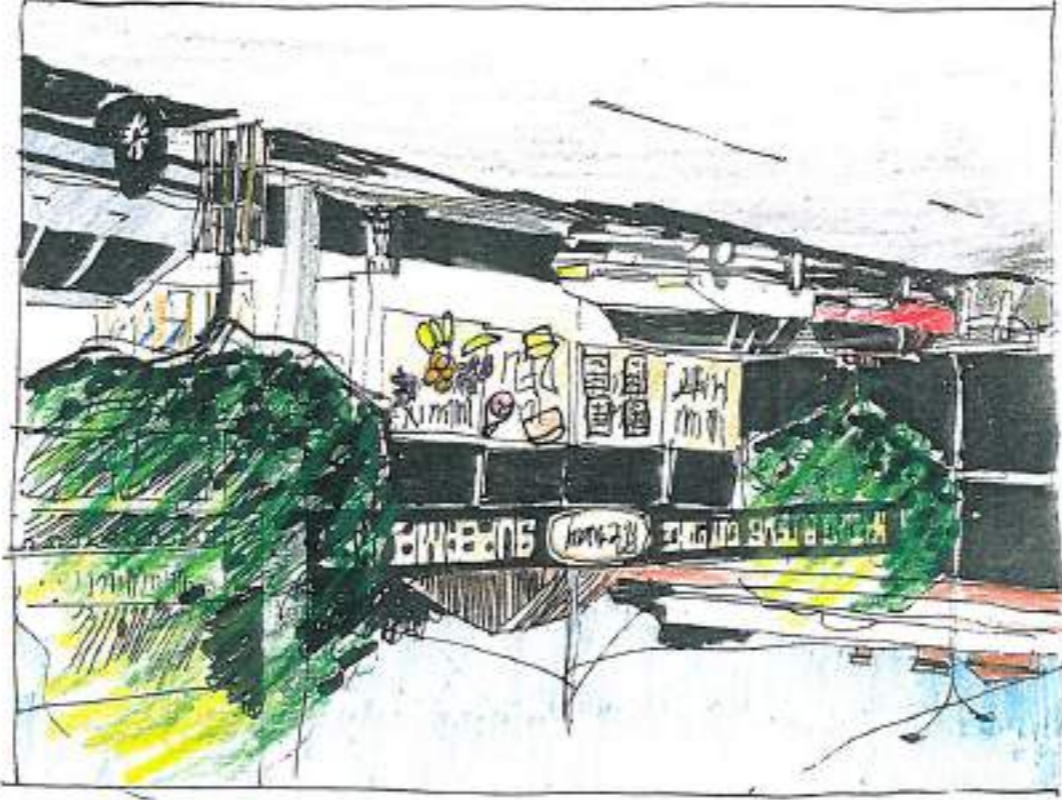




We recommend planting of Oriental Plane Trees in carefully chosen locations at the junction of parking bays.

Note a colourful portrayal of the supermarket produce on the shop front. Traders need to work on better window display imagery in place of words.

**BARKEE STREET NORTH**

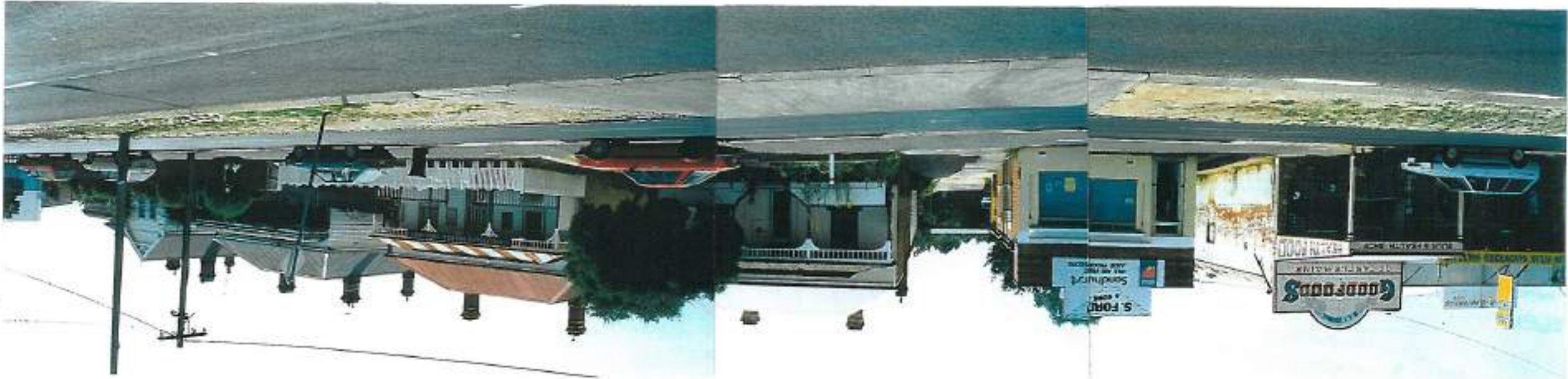
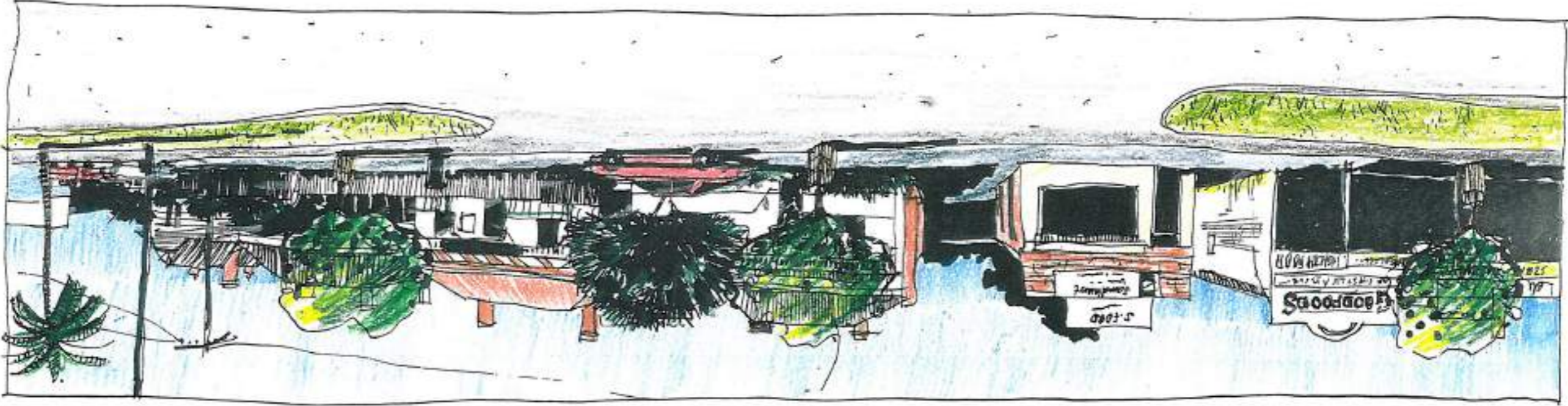


Castlemaine Town Centre Urban Design Strategy



# BAKER STREET NORTH

To improve truck access we would remove a section of the median and surface the median and surface as bitumen roadway. Plant tree planting, including a Cotton Palm in the median will enhance the street and provide shade for cars.



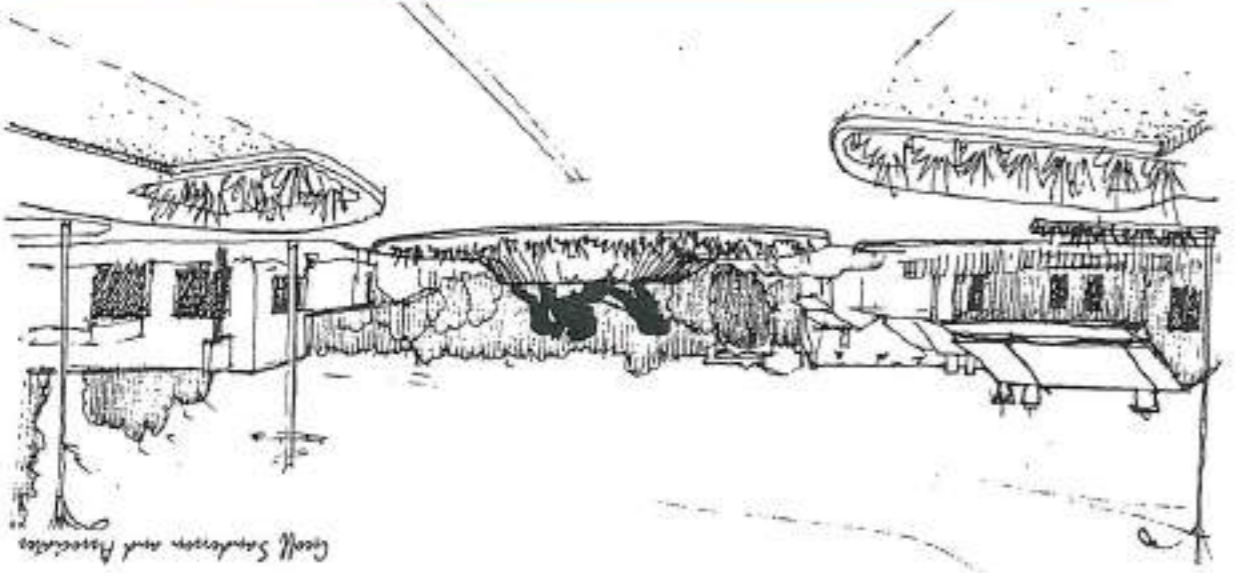
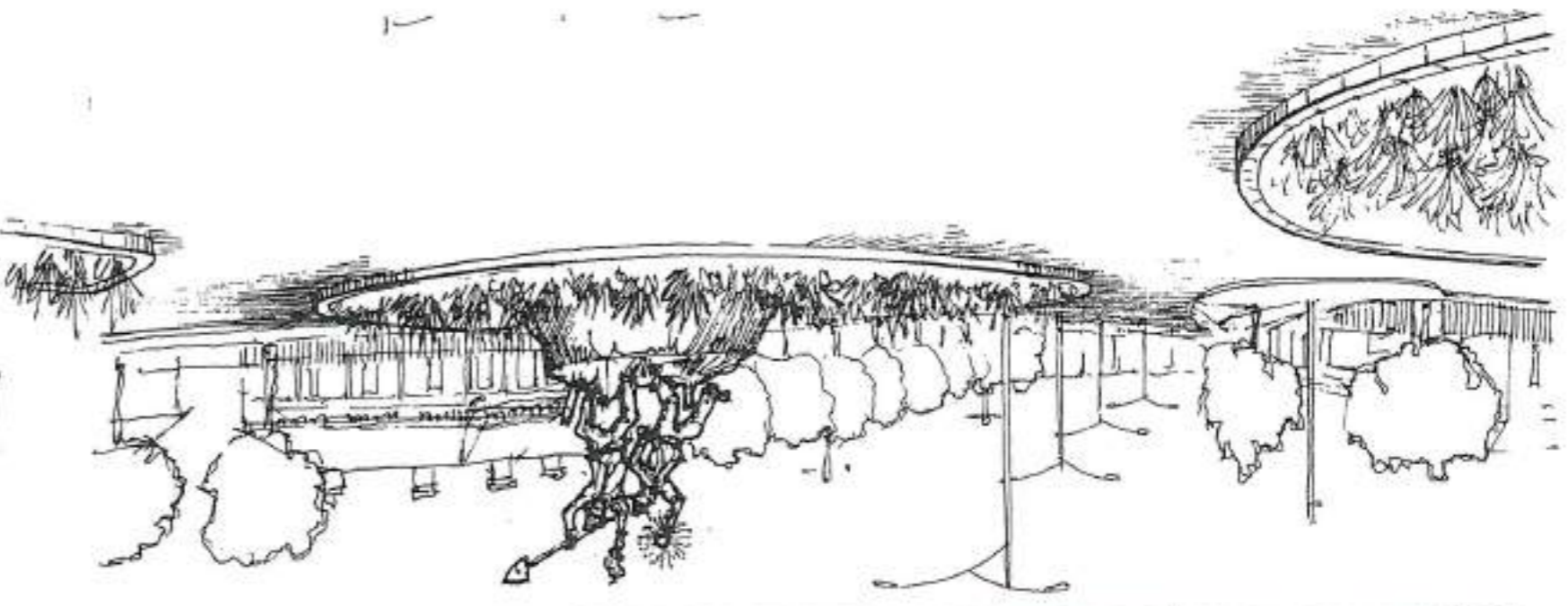
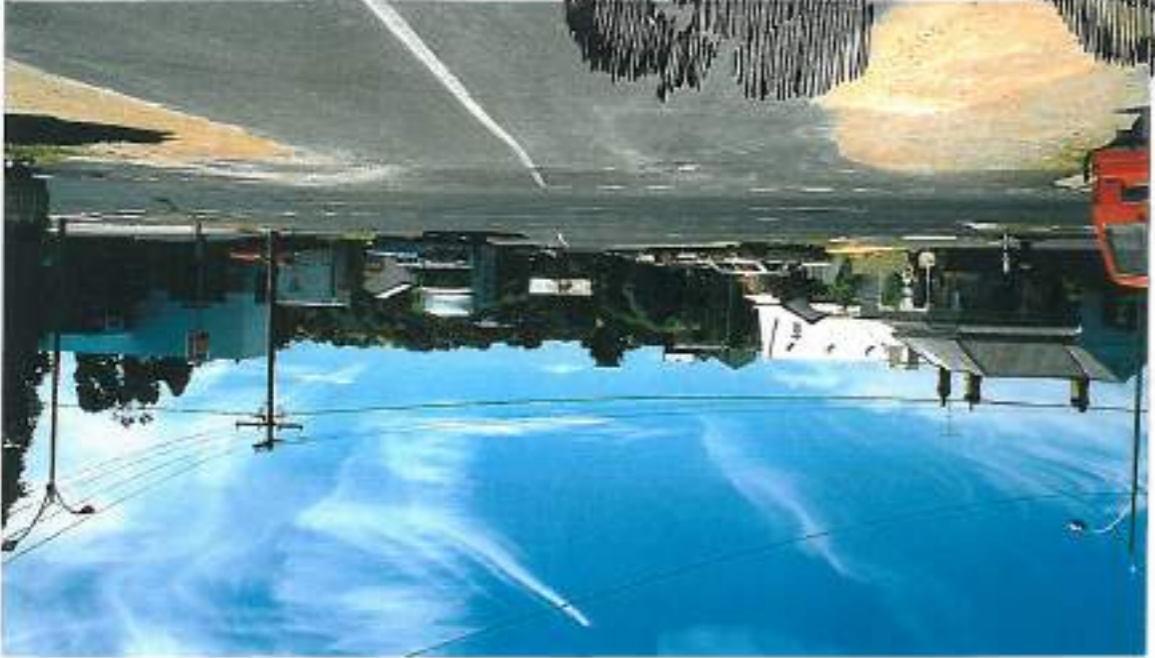
Castlemaine Town Centre Urban Design Strategy

Geoff Sanderson and Associates



We propose a roundabout to herald the entrance to the Town Centre and to slow traffic. The sculpture in the centre of the roundabout is designed to complement the Geoff Hocking sculpture to be placed at the intersection of Forest and Hargaves Streets. The theme is the joy of discovering gold - a deliberately happy thought on arrival in Castlemaine.

**BARBERS STREET/CAMPBELL STREET INTERSECTION**

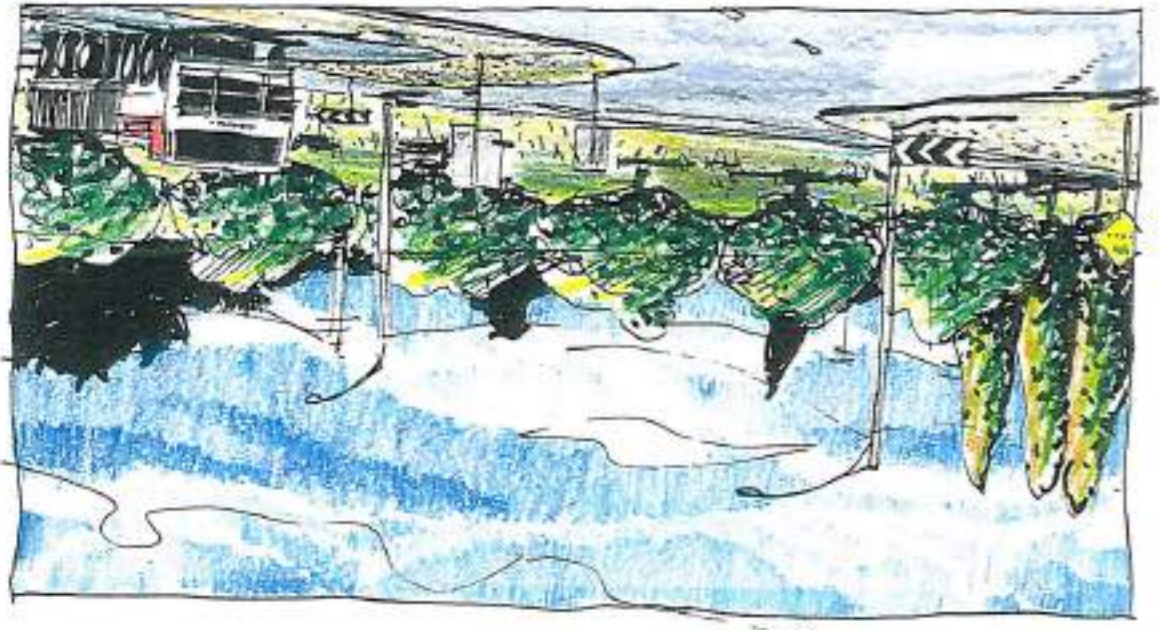


Geoff Sanderson and Associates

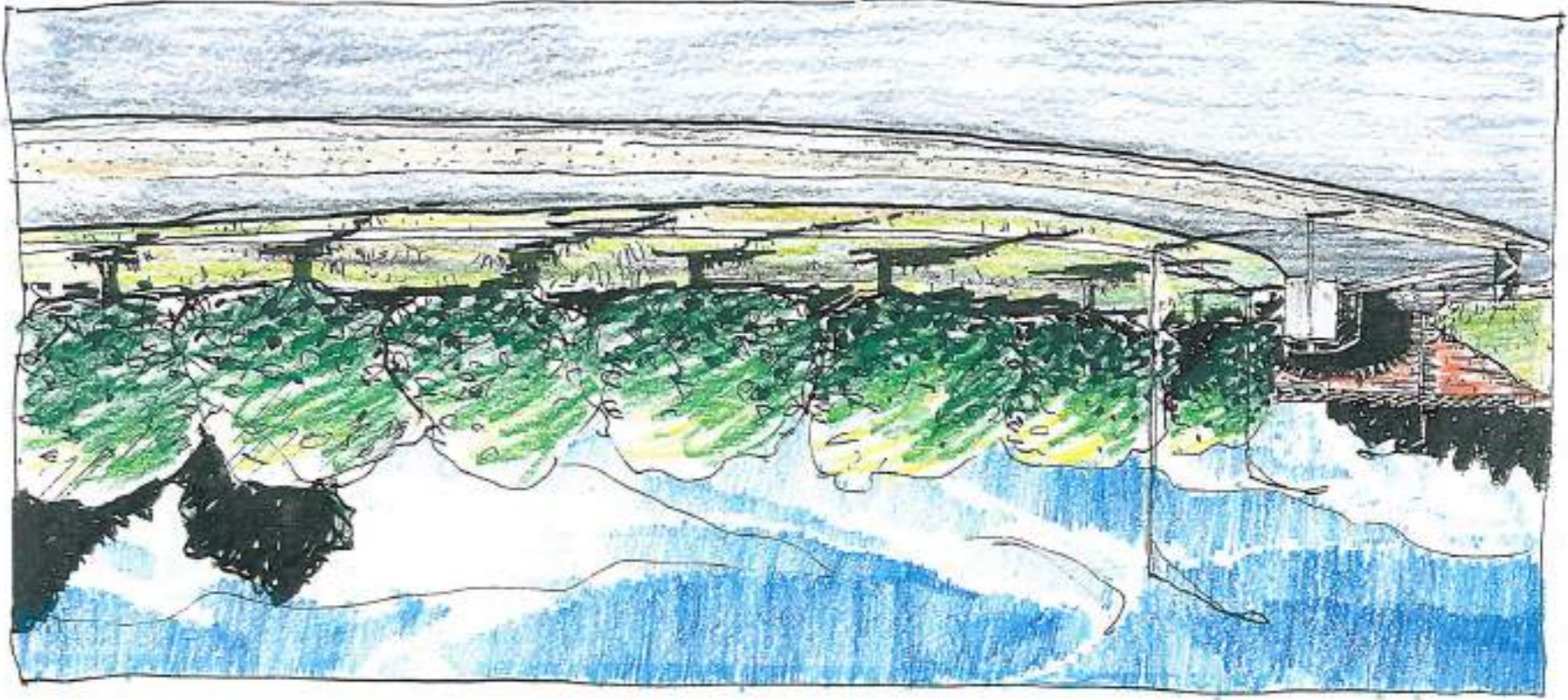
Castlemaine Town Centre Urban Design Strategy



BARKER STREET SOUTH



This is both a major entry to the town and the focus of a view down Barker Street from the north. We propose on the south side planting English Oaks into a slightly regraded area between the road and the railway embankment. On the west side a further row of English Oaks. Consideration should also be given to grassing and irrigating.





There is a feeling of 'forgotten place' about Western Reserve and yet it offers so much to the townscape of Castlemaine. We recommend that it be upgraded and that it function as a Village Green with fairs, entertainment, casual sports events, displays and circuses using it regularly and frequently.

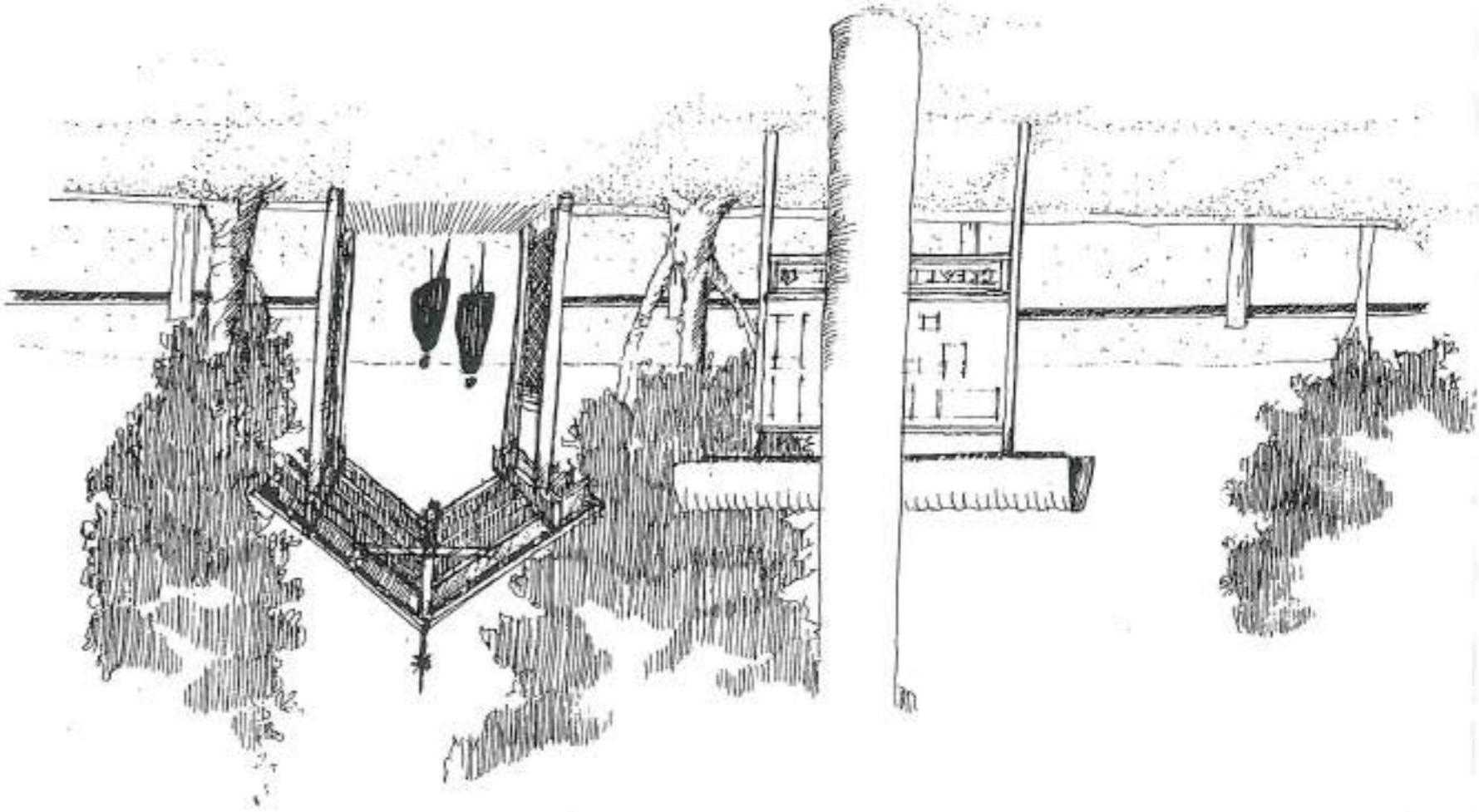
We propose the replacement of the pipe rail fences with a timber and pipe rail barrier, construction of pathways circling the park, picnic areas, play equipment and some parking accessed from Barkers Street. Parking would cater for cars and buses. We also propose a formal pedestrian entry at the corner of Forest and Hargraves Streets. Lighting of pathways and the carpark is also necessary. The park should be accessed from both the Forest St / Hargraves St corner and from a pedestrian crossing on the pathway axis on the west side of the new supermarket. More trees are needed as well as park furniture in keeping with the style of seats, litter bins, signs and lamps recommended within study.



WESTERN RESERVE



FOREST STREET/  
WESTERN RESERVE



This sketch shows a formal entry to the park at the corner of Hargraves and Forest Streets. This intersection will become much more significant when the roundabout, with sculpture, is created. A significant location is an ideal place to show off Western Reserve and 'involve' it more with the town centre, especially the Market Square/Victory Park precinct. The 'Great Dividing Trail' directory board is relocated.

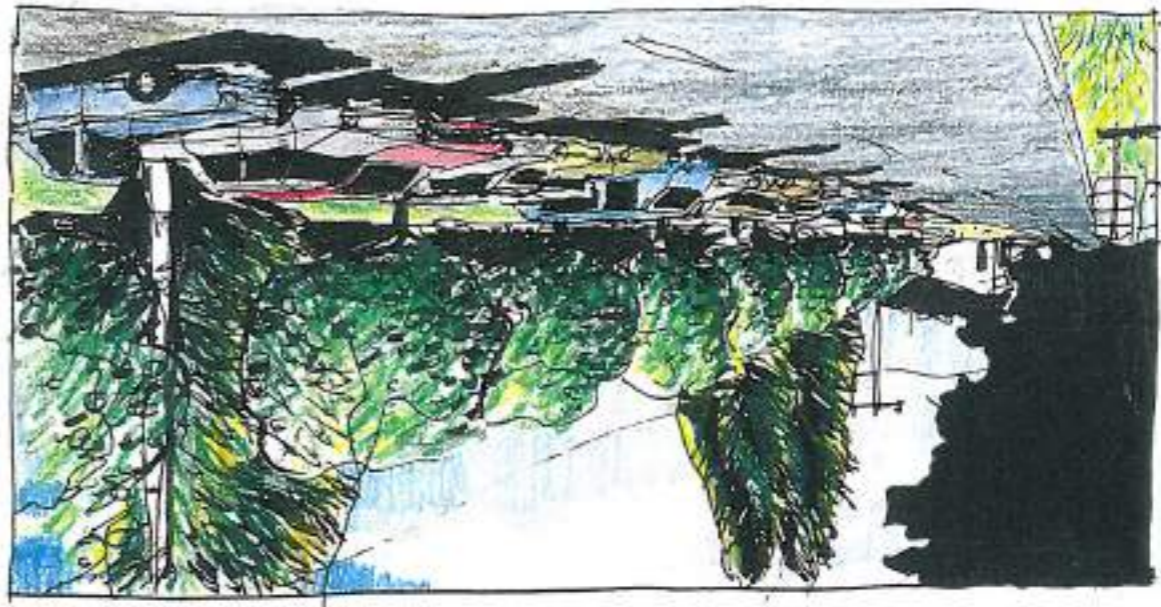


Goff, Sanderson and Associates

Castlemaine Town Centre Urban Design Strategy



**FOREST STREET/  
WESTERN RESERVE**



Where Forest Street passes Western Reserve there is an opportunity to de-emphasise the 'kerb contained' character of the Midland Highway and concurrently increase parking. The sketch shows angle parking, increased tree planting and more of a 'Town Street' feeling.



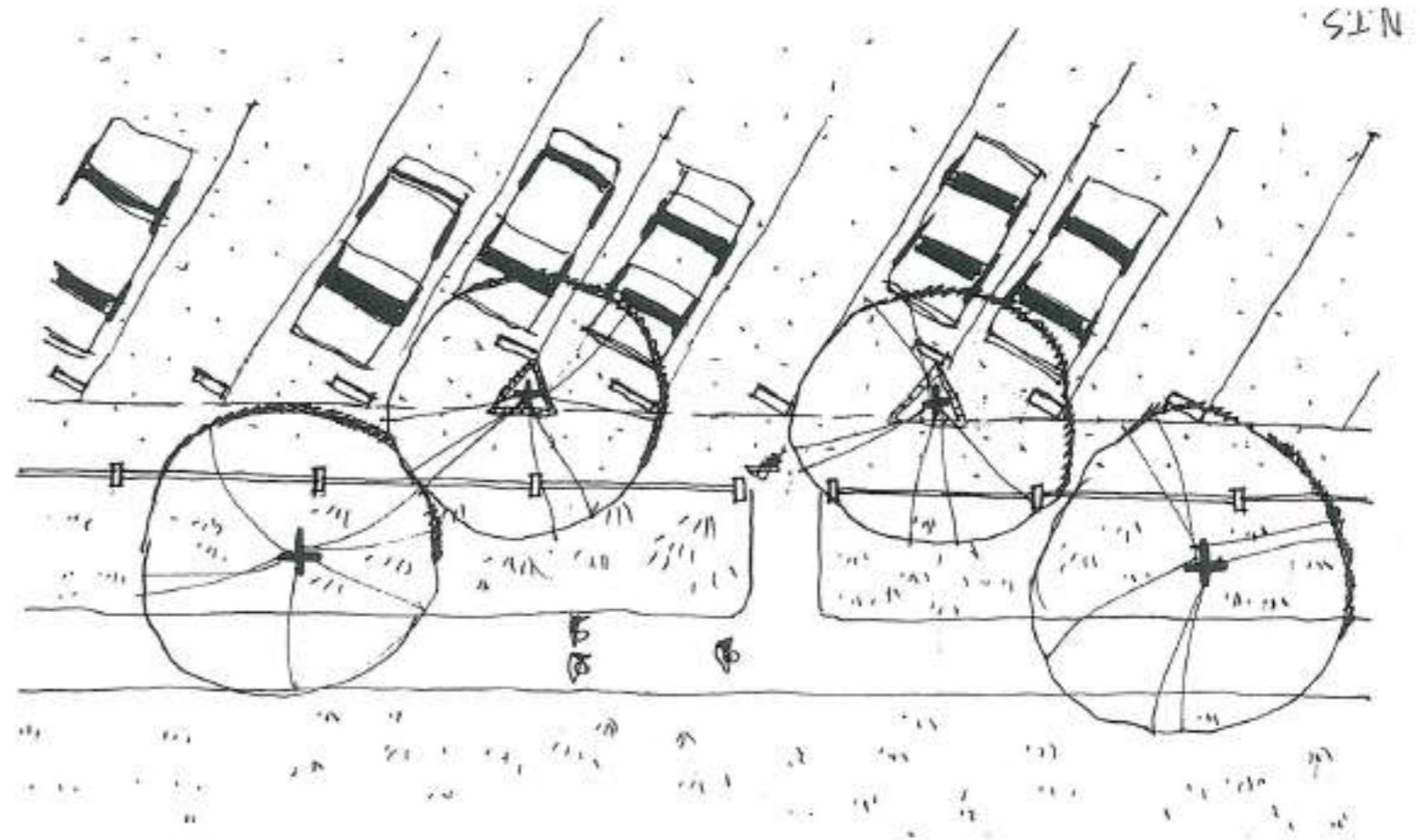


The sketch illustrates a less complicated treatment of the reserve boundary. The posts are off white painted hardwood and rail is galvanized pipe.



FOREST STREET / WESTERN RESERVE

The plan show the Forest Street footpath relocated into the park.



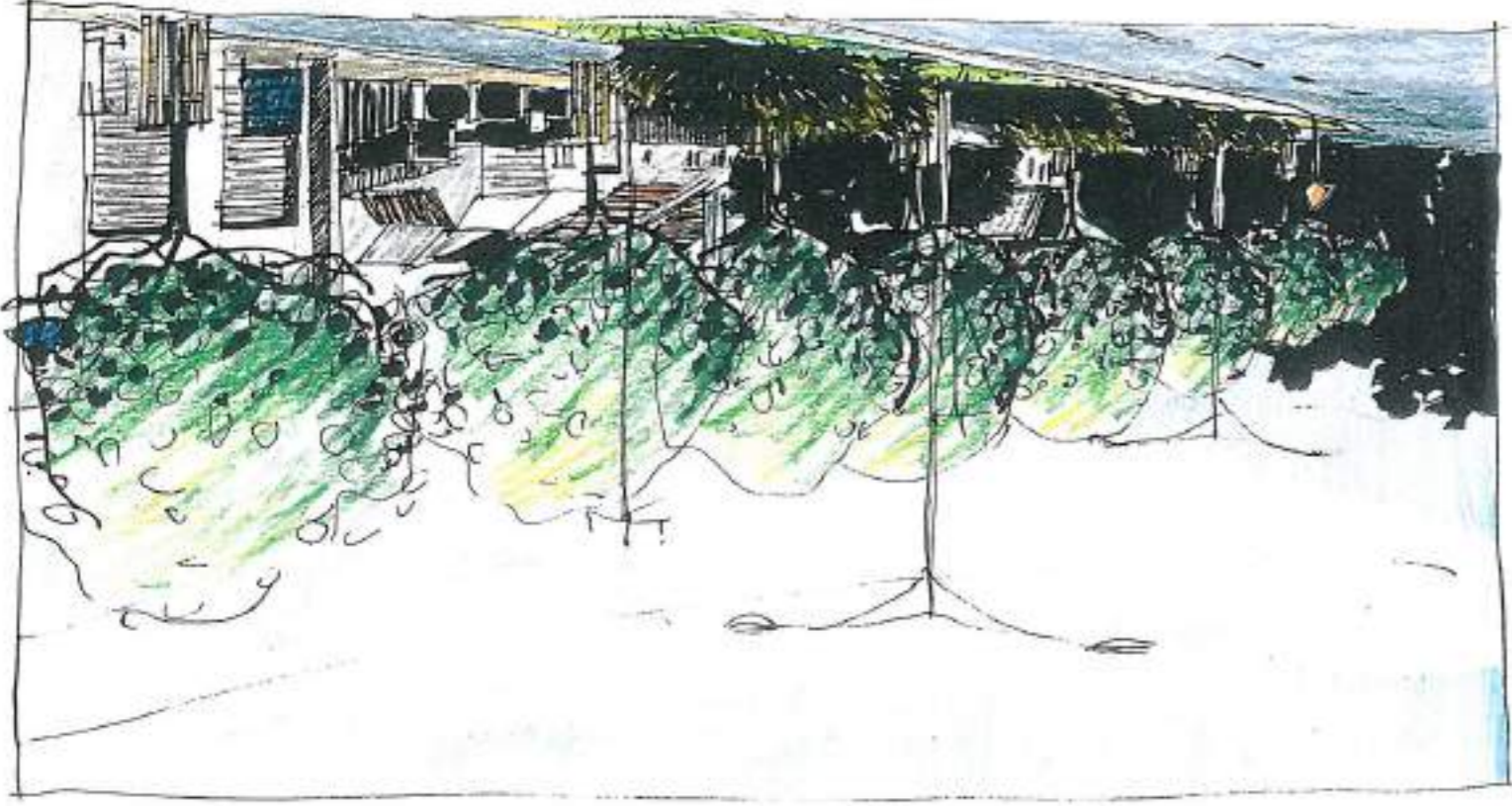
Castlemaine Town Centre Urban Design Strategy



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FOREST STREET

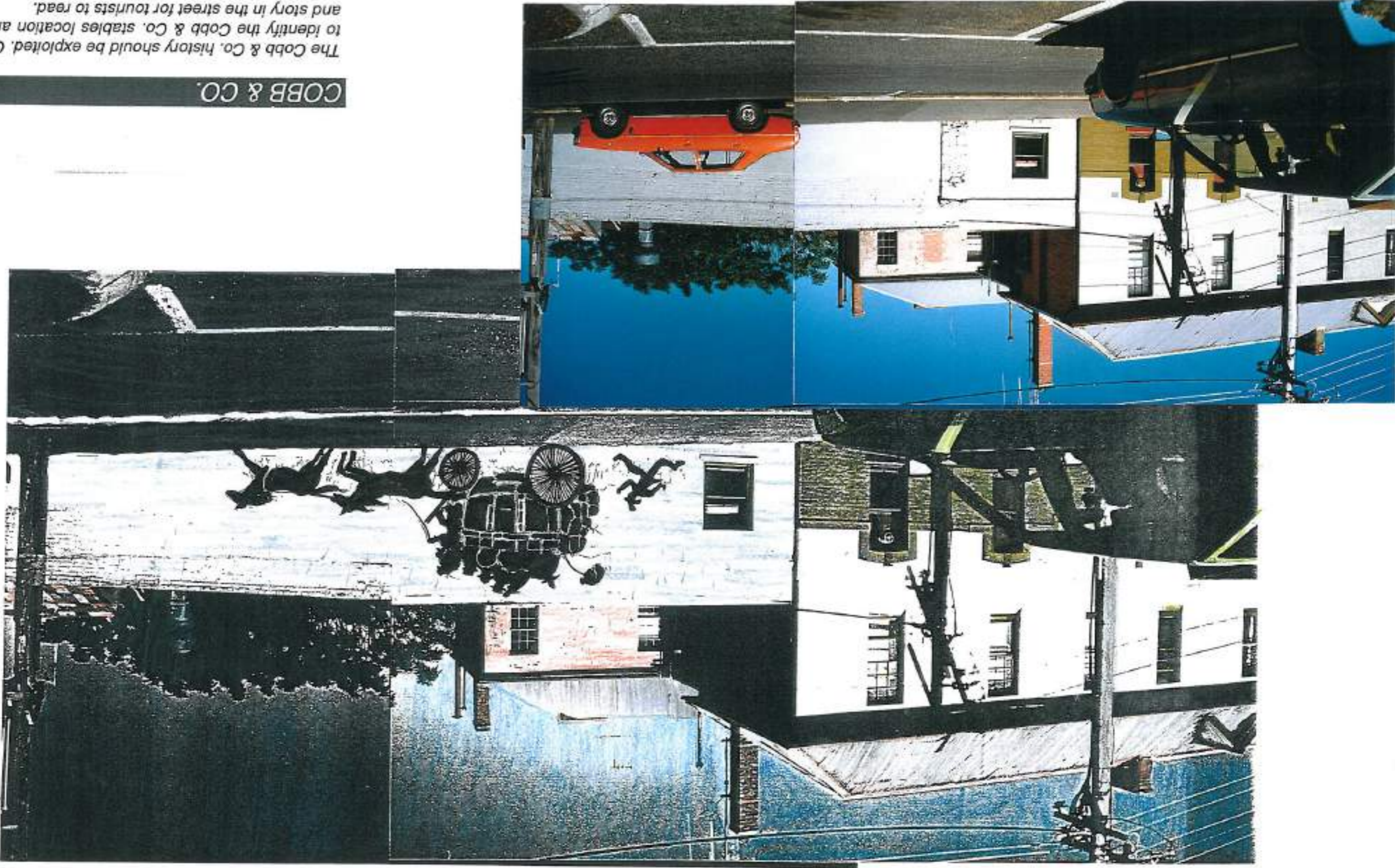


Sketches show the addition of Oriental Plane trees to street - planted in the parking lanes at junction of parking bays.



Castlemaine Town Centre Urban Design Strategy

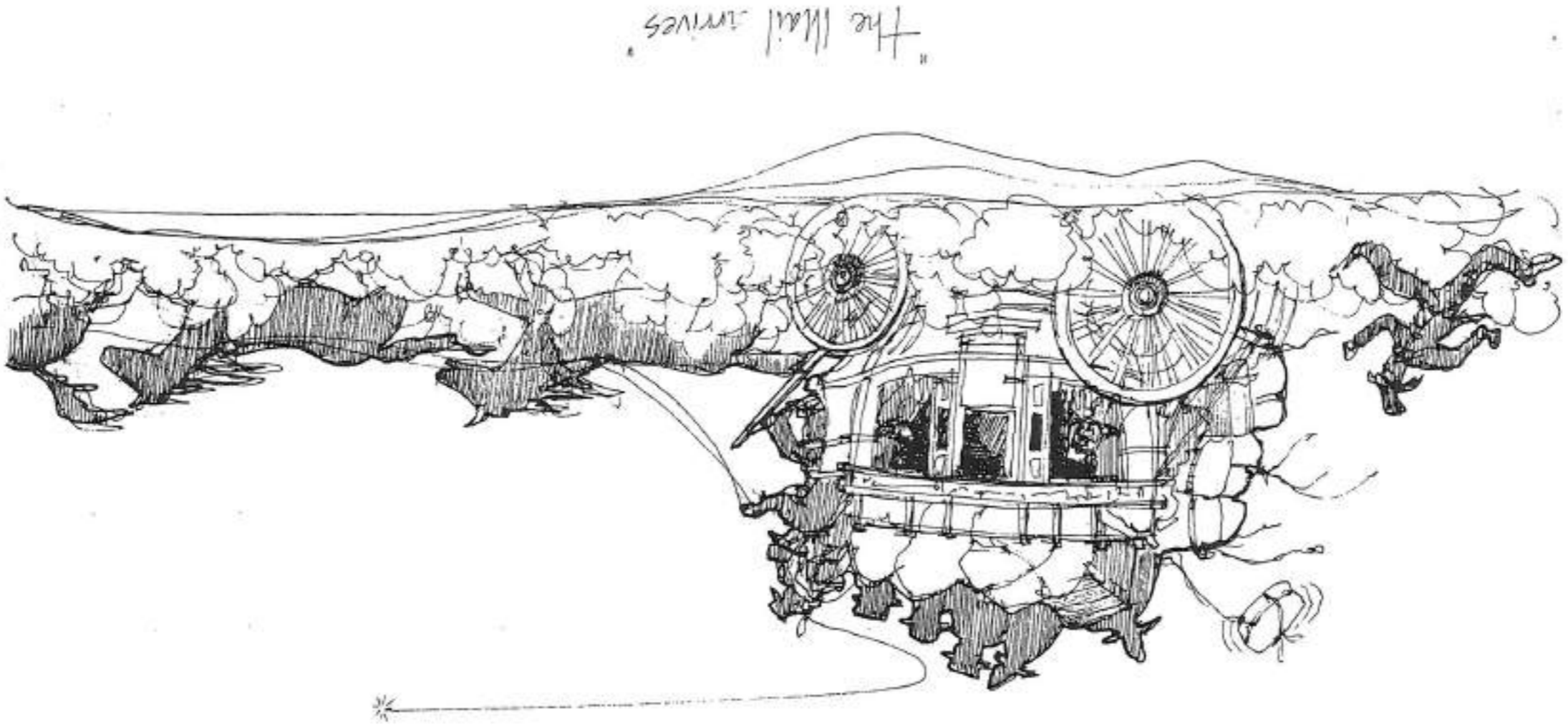




The Cobb & Co. history should be exploited. One idea would be to identify the Cobb & Co. stables location and place a plaque and story in the street for tourists to read.

**COBB & CO.**





"The Mail arrives"

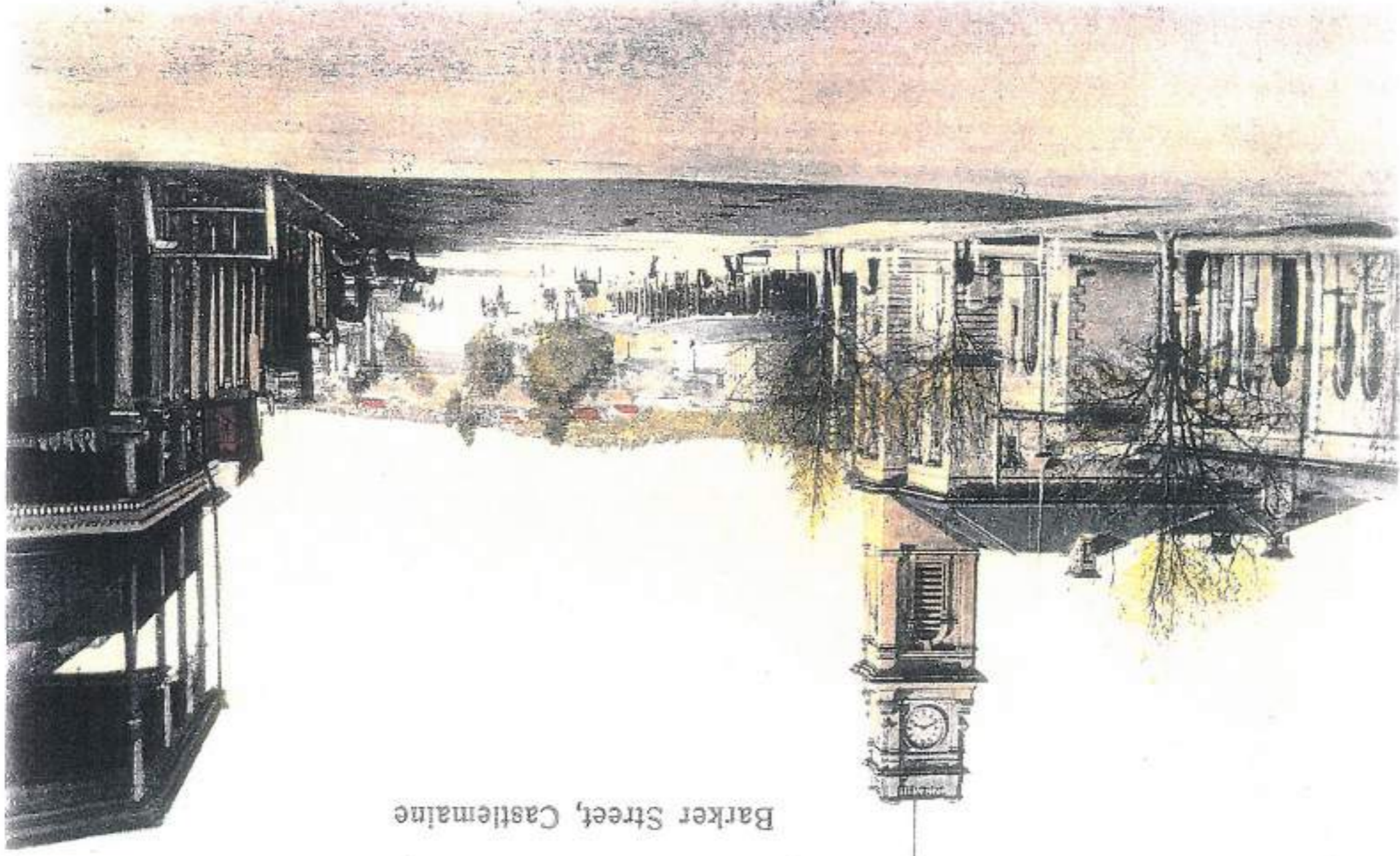
Clever use of sculpture can help to bring Castlemaine's history to life, especially if it has a sense of movement and humour. Cobb & Co. stables were behind this wall but one would never know unless the story is told. It is not intended that the illustration be used - it is intended only to promote an idea.

CRITERION HOTEL AND COBB & CO.



ACKNOWLEDGMENTS  
& REFERENCES



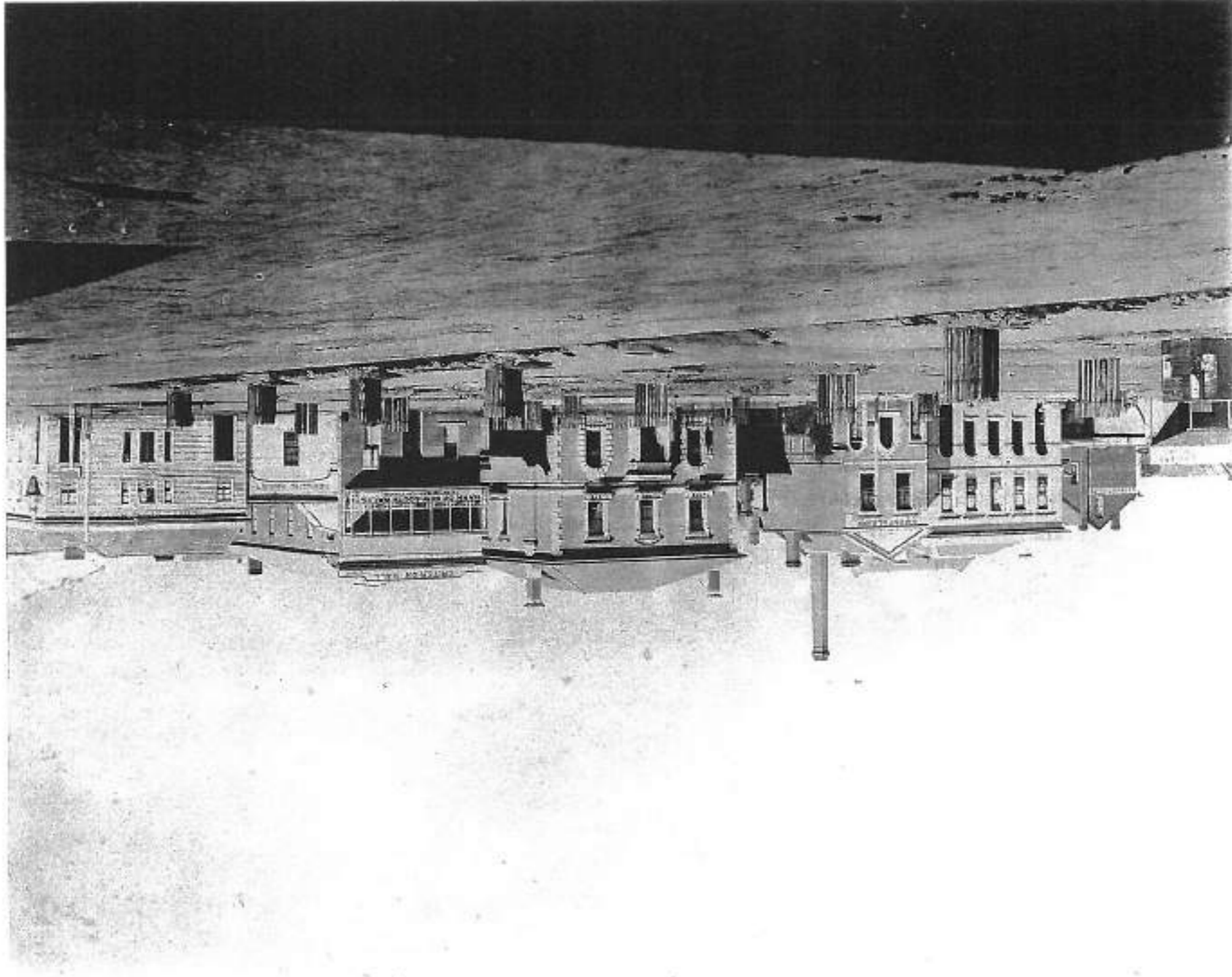


Barker Street, Castlemaine

The following historic photographs were provided by the Castlemaine Art Gallery and Museum. They show various streetscape features and serve to explain the derivation of the street furniture details we recommend retaining. They also show very wide streets without motor cars, and the extent of street tree planting which began in the 1860's.

Photo 1 Barker Street, Castlemaine (1908) looking south from the corner of Templeton Street. Ref p596  
 Note the Oriental Plane Trees on the left hand side, the sawn timber barriers to protect vehicles and persons from falling into the open, stone lined drains.



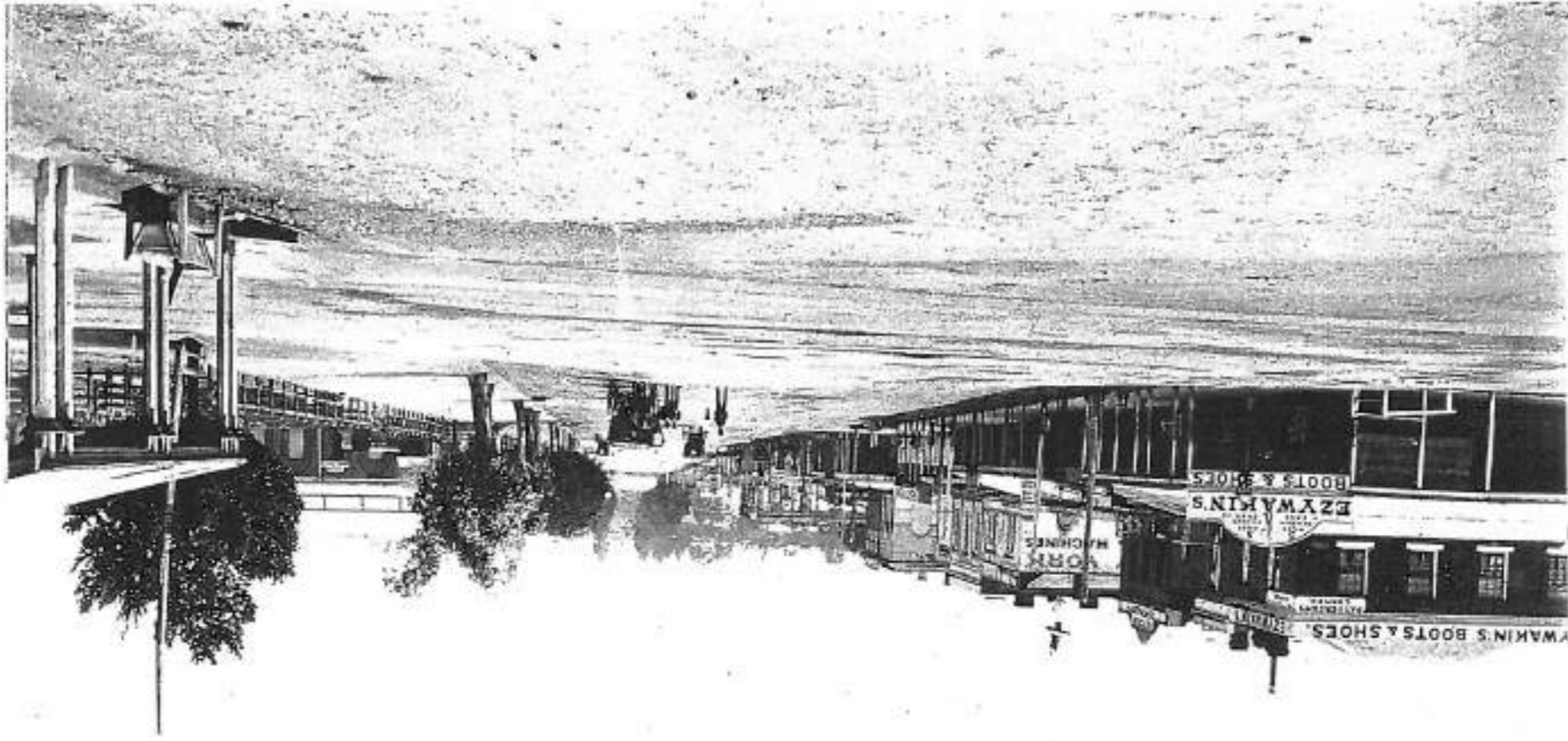


The photo shows the origin of the three sided picket surrounds being used to protect young trees. This detail should continue to be used by Council for all tree planting in vulnerable locations.

Photo 2: Market Square - West Side, now Victory Park. Photo taken from north side of Mostyn Street (c 1865). Ref p. 731

*Geoff Sanderson & Associates*





The photo shows that only one row of trees from the 1865 photograph remains, having been sacrificed for the stock yards. The trees in the photograph appear to be Elms in the background and on the left side (this tree is still in place which indicates that it is approx 133 years old. The trees with lighter toned foliage are most likely Oriental Plane Trees.

Photo 3: Mostyn Street (1890 - 1900) Ref 04331



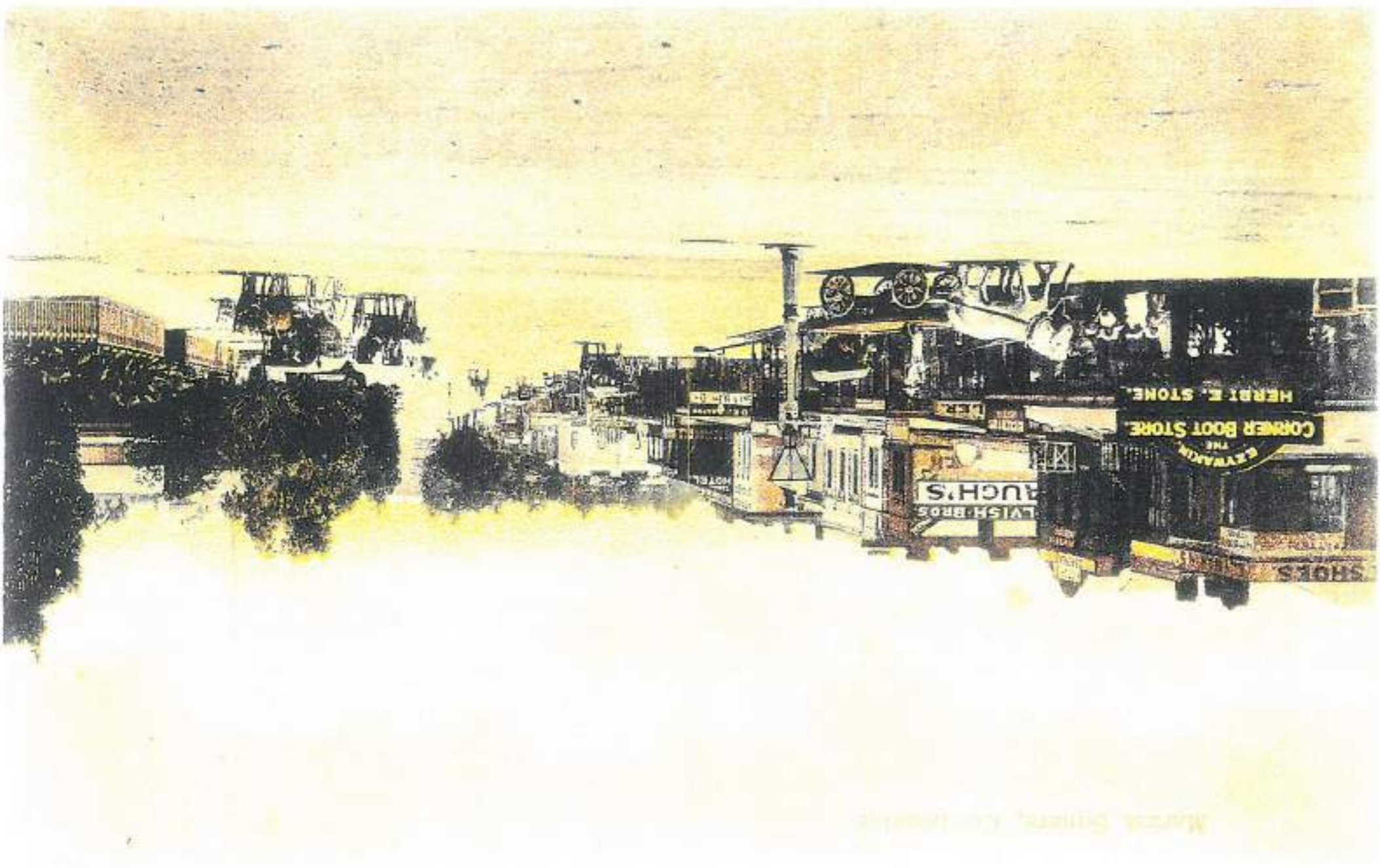
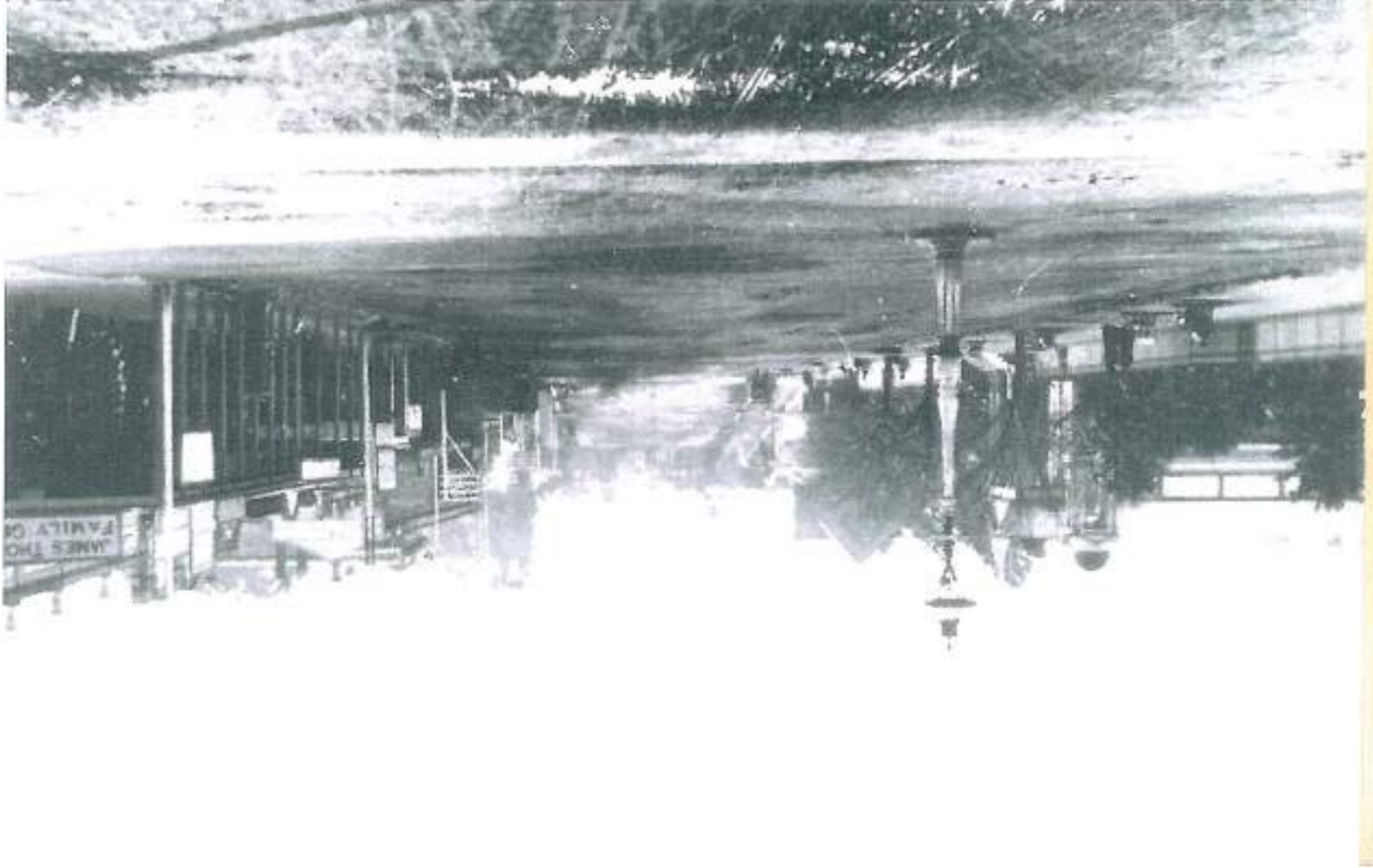


Photo 4: Market Square (c 1905) ref P 1236.

The photo shows Mostyn Street looking east and the healthy Oriental Plane Trees and Elm Trees as well as the young Cotton Palms in Victory Park. The gas lamp at the intersection with Barker Street, the picket fence surrounding the park and the stone lined drains are all gone. The north east corner also has the sawn timber barriers.





Note the seats along the south side. At least 6 seats are visible in this photo and photos of the same section of Mostyn Street taken from the Barker Street corner show another six seats. It suggest there were at least twelve seats on the south side of Mostyn Street and that this was the place to be.

Photo 5: Mostyn Street looking west from Hargreaves Street (c 1920's)



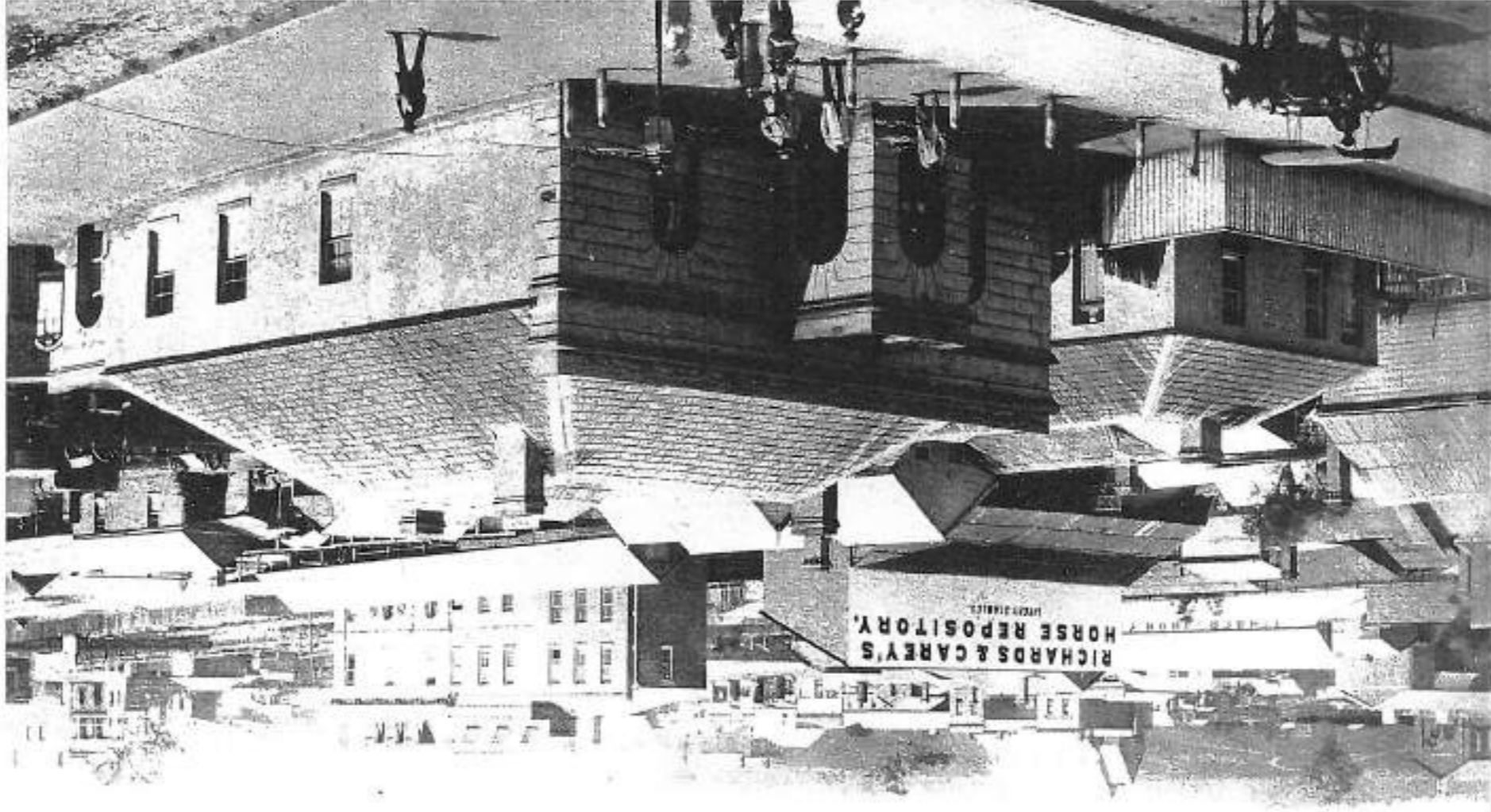


Photo 6: Corner of Hargraves Street and Mostyn Street (c1861) looking north west. Ref P 722.

The rear section of the building remains but not in its former attractive condition. Note the forecourt with its sawn timber bollards and chains. This detail is recommended for the pedestrian outstands instead of cast iron bollards, which were not evident in 19<sup>th</sup> century Castlemaine.



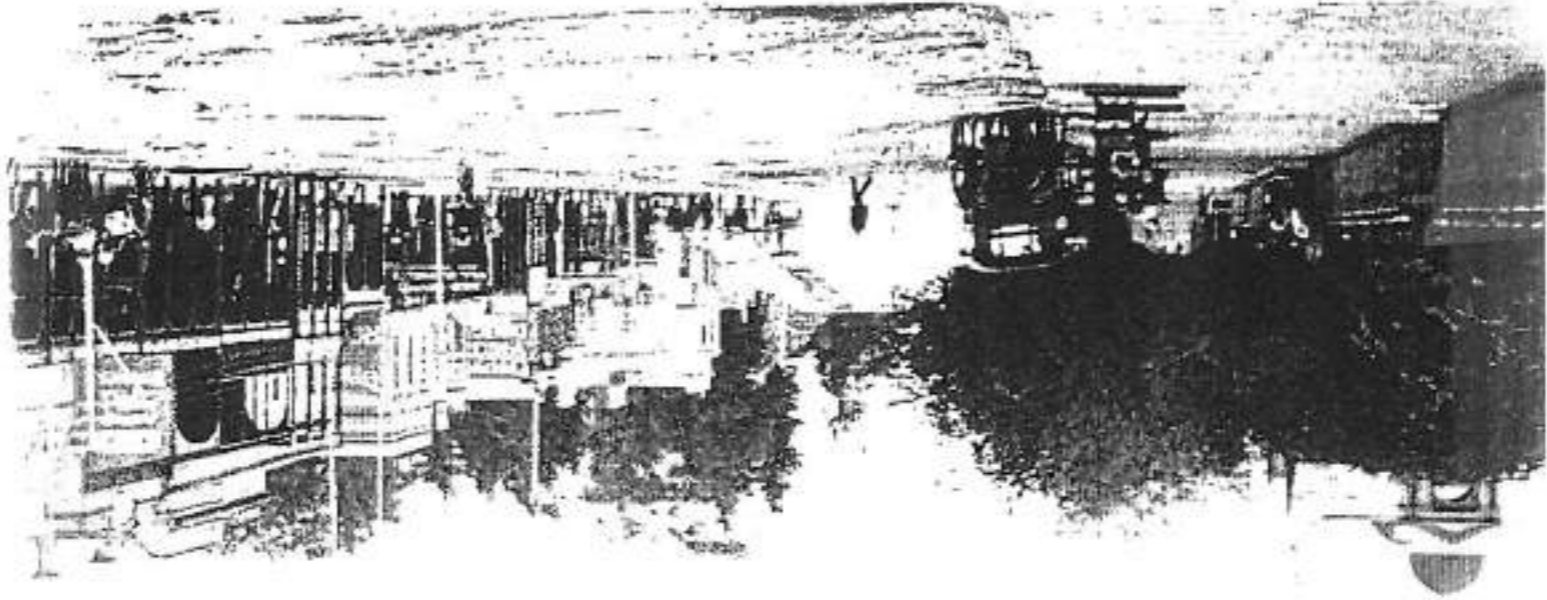
**Photo 7: Lytleton Street Looking West from Hargraves Street corner (c1914) . Ref P 816.12**

Note the gas lamp at the intersection and the substantial Oriental Plane Trees which began life in an unsealed street and with water leaking from the stone lined drains. When the road was sealed, no thought was given to the trees' needs or to the probability that they will damage the pavement. Unfortunately, over time, the needs of the pavement were deemed foremost and most of the trees were removed. Only three of the original planting remain (in front of the Court House) and these must be treated with utmost respect.



Lytleton Street, Castlemaine.

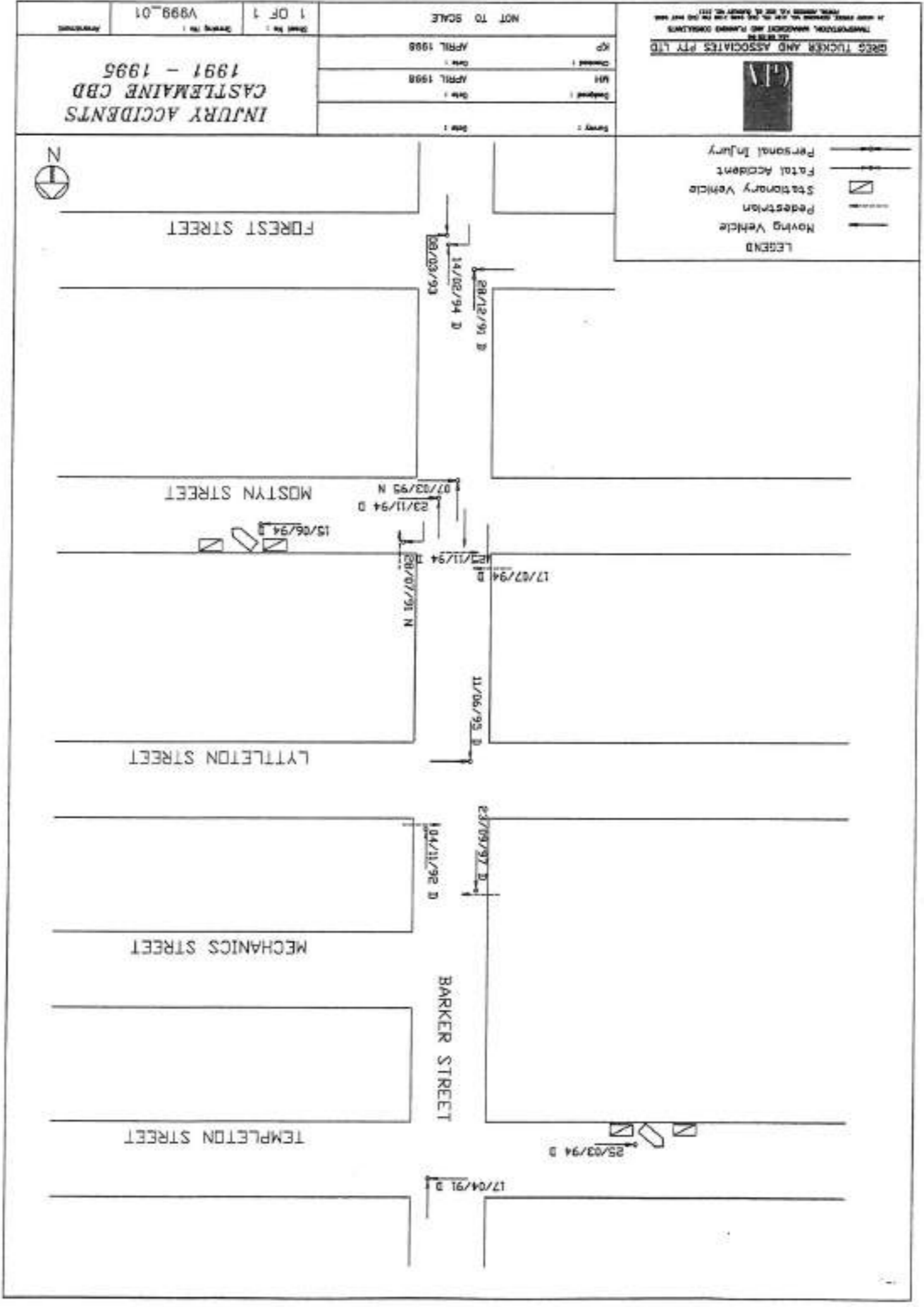




From the enlargement it is just possible to identify the style of seat commonly used in Castlemaine in the late 19<sup>th</sup> and early 20<sup>th</sup> century. These seats in fact had cast iron ends and a centre cast iron leg. They were similar to but longer than those currently in use in Castlemaine. We recommend returning to the original longer style.

Photo enlargement from a Buda collection photo c.1910





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- LEGEND
- Moving Vehicle
  - - - Pedestrian
  - ▣ Stationary Vehicle
  - Fatal Accident
  - Personal Injury

Drawn by : Date : Scale :	1 OF 1 APRIL 1998 NOT TO SCALE
Checked by : Date : Scale :	APRIL 1998 APRIL 1998 NOT TO SCALE
Designed by : Date : Scale :	APRIL 1998 APRIL 1998 NOT TO SCALE
Drawn by : Date : Scale :	APRIL 1998 APRIL 1998 NOT TO SCALE

**INJURY ACCIDENTS**  
**CASTLEMAINE CBD**  
**1991 - 1995**

Forest Street  
 Mostyn Street  
 Lytleton Street  
 Mechanics Street  
 Templeton Street  
 Barker Street

08/03/93  
 14/02/94  
 28/12/93  
 07/03/93  
 23/11/94  
 15/06/94  
 28/11/94  
 17/07/94  
 11/06/93  
 23/09/97  
 10/11/92  
 25/03/94  
 17/04/91



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Ian Huxley and Kevin Walsh

***References***

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The Royal Botanic Gardens Melbourne A history from 1845 to 1970 - R.T.M. Prescott - Oxford University Press  
Castlemaine Town Centre Townscape Improvement Plan April 1989 - Trevor Westmore  
Castlemaine Township, Market Square Precinct - Traffic Report - Jan 1997 - Andrew O'Brien & Associates P/L  
Castlemaine Town Centre Parking Study - Parking Report January 1997 - Andrew O'Brien & Associates  
Victory Park Castlemaine - Conservation Management Plan - January 1997 - Allom Lovell & Associates with John Patrick Pty Ltd.

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Rob Mitcheilree - Graphics and design details  
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Simone Brott - Technical support

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